

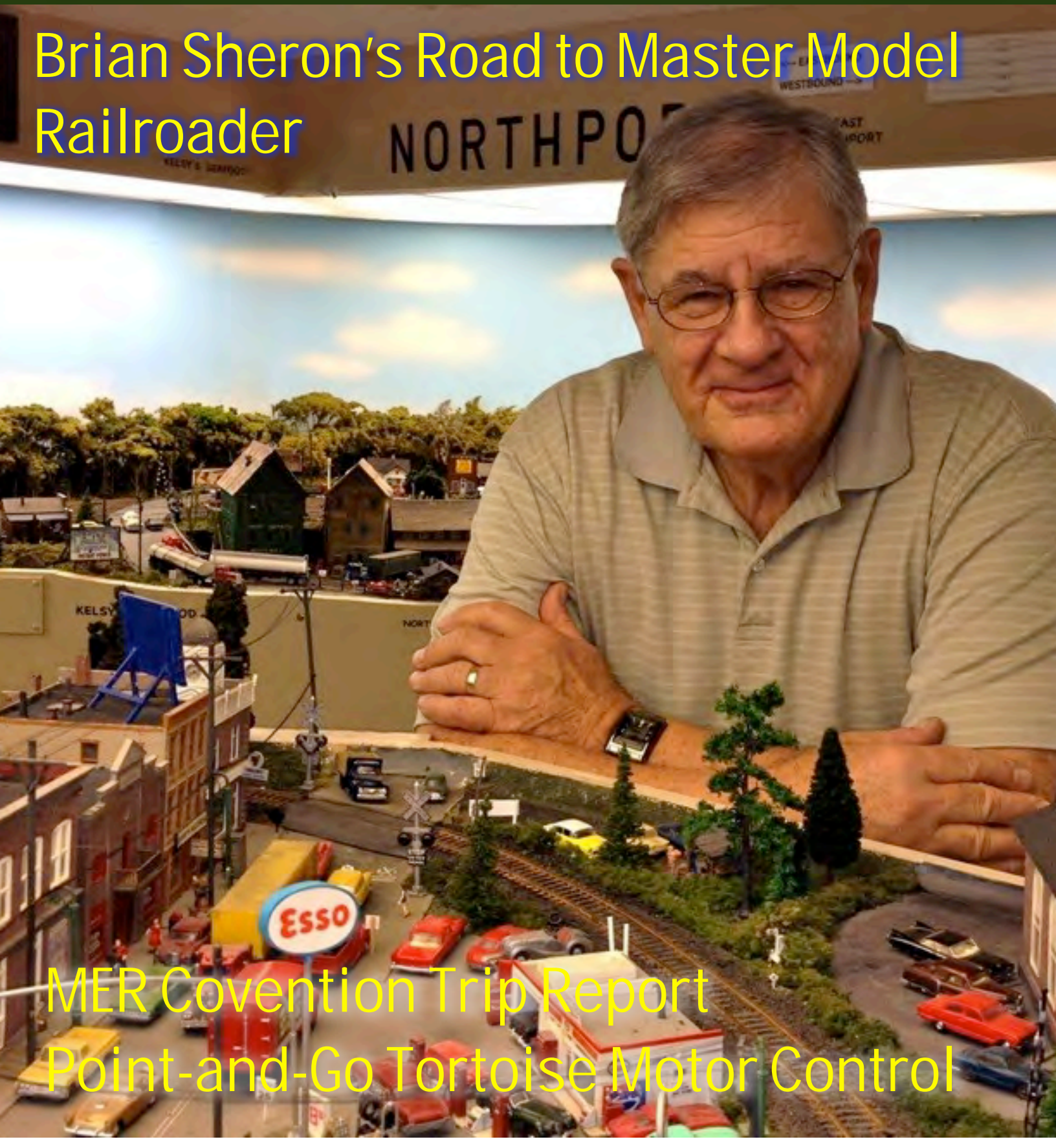
The

POTOMAC FLYER

Jan-Mar 2012

Winter Quarter

Brian Sheron's Road to Master Model Railroader



MER Covention Trip Report
Point-and-Go Tortoise Motor Control

The Division Crew

Superintendent
Marshall Abrams
 301-588-1005
Abrams_railroad@comcast.net

Senior Asst. Supt.
Bill Day
 703-406-4112
billday22@verizon.net

Asst. Supt.
Brian Sheron
 301-349-5754
BWSheron@mac.com

Paymaster
Mike White
mm.white@comcast.net

Clerk
Andrew Dodge
 301-774-7753
orocity@hotmail.com

Achievement Program
 Coordinator
Brian Sheron
 301-349-5754
BWSheron@mac.com

Database Administrator
Mike White
Potomac_NMRA@comcast.net

Potomac Flyer Editor
Marty McGuirk
 703-331-1285
mjm McGuirk@comcast.net

Layout Disposal Assistance
Clint Hyde
 703-803-3068
chyde@cox.net



The Potomac Division, Mid-Eastern Region, National Model Railroad Association includes the District of Columbia; Calvert, Charles, Montgomery, Prince George's and St Mary's Counties in Maryland; Arlington, Fairfax, Fauquier, Loudoun, Prince William, and Rappahannock Counties in Virginia, as well as all area independent cities.

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The Potomac Flyer

Potomac Division's Quarterly Newsletter

Submission Deadlines	Advertising rates:
Spring Issue March 1	1/8 page business card
Summer Issue June 1	\$30 per year
Fall Issue September 1	1/4 page \$15/issue
Winter Issue December 1	1/2 page \$25/issue
	full page \$45/issue
	Back cover, half page \$45/issue

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From the Business Car

Marshall Abrams
 Division Superintendent

Abrams_railroad@comcast.net



I was looking forward to participating in the MER Convention in Cary, but doctor's orders made that impossible. Among other disappointments, I had to cancel out of presenting two clinics. I was impressed with the flexibility of Jack Dziadul, the organizer of the clinic track, who graciously acknowledged my inability to participate and filled in the gap with repeats of other clinics. I hope we operate so seamlessly and professionally when we host C&P Junction 2013.

Concerning C&P Junction 2013, the site selection team of Bill Demas and Kurt Thompson has recommended a hotel. We're looking at the costs and budget and hope to negotiate a better deal. If you'd like to participate in the planning and organization of the convention, please get in touch with me at the e-mail address above.

The White Flag Extra clinic program is doing well. The last session was rated by all who attended a 10 out of 10. It doesn't get any better than that! White flag extra are planned for Saturday, January 21, 2012, at Club House Number 2 at Leisure World in Silver Spring; and Saturday, March 3, at the Reston Regional Library, 1125 Bowman Towne Dr., Reston, Va.

The Minicon is coming on March 31, 2012 at South Columbia Baptist Church in Howard County, MD. This is the same location as the 2012 Minicon and again is co-sponsored with our friends from the north in the Chesapeake Division. Due to the small size of the classrooms in last year's and this coming venue, we've decided to hold the clinics as a plenary session in a large room instead of scheduling parallel tracks. We think this should improve the experience for everybody.

Nominations for the Board of Directors are in order. The Board gets elected at the Minicon. The nominating committee selects a slate and also accepts nominations from the members. The Bylaws specify that "any three members may nominate someone via a written notice to the Nominating Committee, with the permission of the nominee. Such nominations must be submitted to the Nominating Committee at least thirty days ahead of the election." Email serves as written notice. February 29 is thirty days before the election. The members of the nominating committee are Brian Sheron <bwsheron@mac.com>, and Tom Brodrick t.brodrick@verizon.net. The Board elects the Superintendent, Senior Assistant Superintendent, Assistant Superintendent, Clerk and Paymaster—usually at a brief meeting held after the election at the Minicon. Another brief meeting is the Division Business Meeting. One item of business will be presentation of his MMR Certificate to Brian Sheron. Brian also missed the MER convention because he was planning to drive with me. If you have any items for the Business Meeting, please let me know.

WANTED: Layout Hosts!

The Potomac Division sponsors layout tours approximately every other month. Layouts don't have to be complete, as layouts in various stages of construction are often just as interesting and informative to members. If you would like to host an open house on a Saturday afternoon and let other members and friends of the Potomac Division see your layout and enjoy your handiwork and modeling talents, please contact the Division's Layout Tour Coordinator, Brian Sheron (BWSheron@mac.com), and let him know you are interested in hosting an open house.

Our Cover Story

Brian W. Sheron, Master Model Railroader #469

Editor's Note: I'd like to offer my congratulations to Brian Sheron, who was officially recognized as MMR #469 at the MER Convention in Cary this past October. Here, in his own words, is Brian's description of the path that brought him from experienced, but burned out, hobbyist to MMR:

Like most model railroaders, I was introduced to model trains as a child, and the fond memories and fun I had provided the impetus for me to get back to model trains when my oldest son (who is now 36) turned 5. While model trains did not stay in his blood, they remained in mine.

So what got me interested in the Master Model Railroader program anyway? Well, I've been working on my third layout (The Long Island Rail Road, Port Jefferson Branch, circa 1964) since we moved into our third and current house in 1988. My layout was fairly complete by 1996, and was a featured article in the September, 1997 issue of Railmodel Journal magazine.

I actually "burned out" on model railroading about that time (as I told people, one too many drops of solder fell on my ankles

while I sat cross-legged under the layout wiring up turnout machines), and turned my attention to learning to play the banjo and restore classic wooden boats as hobbies. However, in 2004 the Potomac Division's layout tour coordinator asked if I would be willing to host an open house/layout tour. I was at first hesitant, but

decided it was time to get back to the railroad, and I therefore agreed. I cleaned up the layout and got it back to a sufficiently operational condition for the open house. Once back into the hobby, I discovered and became fascinated with DCC and sound locomotives. This led to me to completely rewire and convert my layout for DCC operation. I also

finally cleaned out a room in the basement adjacent to my layout room, and was able to expand my railroad to add the New York City Pennsylvania Station terminus. Following these improvements, I was again asked by the Division to host an open house in May of 2008. Subsequent to the open house, Tom Brodrick, a member of our Division who had visited my layout, asked me if I'd like to join him and others in his group in an operating



session at his house. I had never participated in an operating session, didn't know what to expect, or if I'd like it, but figured I'd give it a try.

I went to the session with Tom and his operations group, and had a great time, since it opened up a whole new aspect of model railroading for me. I assume I didn't offend anyone or screw up too badly, since the group invited me back, and by July I was asked to host my first operating session. It turned out that a number of the members in the operations group were also active in our division, including the division superintendent, Marshall Abrams. In October of 2008, Marshall approached me and asked if I'd like to be the Achievement Program (AP) Coordinator for the Potomac Division. I said "Sure. What's an Achievement Program?"

Well, I quickly familiarized myself with the NMRA's Achievement Program by gathering and reading all the information I could about it and learning what my duties were. I also found out that as the AP coordinator, I had to write a column for the quarterly division newsletter. But what could I write about? Figuring I could kill two birds with one stone, I decided that the best way to learn about the Achievement Program was to participate in it and try to earn Achievement awards. That way I could use my learning experience as a source of information for my quarterly column. Not quite 3 years later, I completed the requirements for the Master Model Railroader (MMR) certificate.

I think that if working towards achieving my MMR certificate taught me anything, it was to

become a better modeler. Working through the requirements was a challenge. But challenges are good. They force you to do better and achieve results of which you probably thought you weren't capable.

Brian earned the following certificates to achieve his Master Model Railroader designation: Author, Volunteer, Model Railroad Engineer-Civil, Model Railroad Engineer-Electrical, Chief Dispatcher, Master Builder-Cars, Master Builder-Scenery

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MER Convention Report

MARTY MCGUIRK

Usually I have lots of scheduling conflicts in October so I can't plan to go to the MER Fall Convention, but this past October the weekend was open, so I made a very last-minute trip to Cary.

Model railroad conventions can run hot or cold, and I've been to plenty of regional conventions over the years. Frankly, some of them have been, well, awful. But not this one.

First of all, the show went off without a hitch. The hotel (Embassy Suites) was excellent and even the food in the in-house restaurant was pretty good and reasonably priced. I obviously didn't get to see everything, and I'm sure I missed some great layout open houses and clinics.

The clinic lineup was outstanding. They ran four rooms from 8:00 until 11:00 pm for three days, which is quite a feat! Friday was pretty windy and rainy so I stayed inside, and judging from the attendance in the clinic rooms, I wasn't the only one!

Bruce DeYoung gave two excellent clinics, one on slate roofs and the other on "Modeling Tricks and Tips." Here's Bruce showing one of his tips, a great way to spread ballast:



Daniel Fisher is a structural engineer who specializes in steel-framed buildings. He gave an "extra fare" clinic on modeling steel framed structures. For \$12 I got a GREAT handout and a



small kit to build a steel-frame shed. Dan displayed this model, which is a Walther's Car shop with complete framing and bracing added to represent a steel-framed building with brick curtain walls. Apparently, Dan's factory manufactures droids!

After lunch on Saturday, I headed over to check out Bruce Faulkner's N scale CSX layout. He has a CTC signaling system installed, but is only now really starting to make progress on the scenery. It looks like a great railroad to operate and I may need to get a few of the locals from this area to make it our next operating road trip. Here's the large freight yard Bruce's layout:



All in all, a great weekend seeing old friends and making new ones!

Around the Division

Ghost Model Railroads of the National Capitol Region

BOB ROSENBERG



Most of us are particularly busy during the month of December, what with all the family and holiday activities going on, so it took some effort on my part to squeeze in a trip to the Mount Vernon area of Alexandria for a very different type of Potomac Division home layout tour. This one had

been arranged by Clint Hyde and was unique in that the builder/owner of the railroad, Tory Martinelli, had been deceased for several years and we were there not to run or even see his creation but to purchase any of the remaining locomotives, rolling stock, unbuilt kits, or structures that might be of interest to us prior to its being dismantled. I particularly wanted to see it because I had been advised by Bill Mosteller, another Potomac Division member, that the railroad was set in New England, the part the of the country that I'm from and the area that I had chosen to model myself.

Tory Martinelli was a retired captain in the Navy Civil Engineering Corps, and his construction techniques reflected his professional expertise. It wasn't a large railroad, but it was beautifully crafted and his penchant for meticulous detail when it came to his structures and scenery made it seem bigger than it really was. He was, without question, a very talented modeler and most of us, me included, could have learned a

Thoughts on a Ghost Layout

Tory's layout was indeed quite interesting. There are some photos of it on the [Potomac Division Yahoo Group site](#), for those who want to look. Even the underside was well done, benchwork assembled with screws, classic L-girder, etc. I'd met Tory a couple of times about 10 years ago, at mini-conventions, I did not know what he was doing regarding a home layout, don't recall talking to him about such a thing, etc. I wish I had, now...

Around that same time, after I had helped a few widows to dispose of the layout left behind by their husbands, I also wrote an article, saying essentially two things:

If you have one of these "ghost layouts" as Bob called them, please have an open house sometime. I really don't want the first time I see your layout to be when your widow calls to ask if I can help dispose of it. That's not nearly so enjoyable for me or anyone else as opposed to your being there to talk about it.

Make sure that your spouse knows how and who to contact about getting that help when the time comes that he/she needs it. At the moment, that is me specifically, but eventually I won't live in this area, and someone else from Potomac Division will be the right person. Make sure they know at least how to contact the Division. You can also give them NMRA Headquarters contact info, because HQ will send any such request to the local division. - *Clint Hyde*



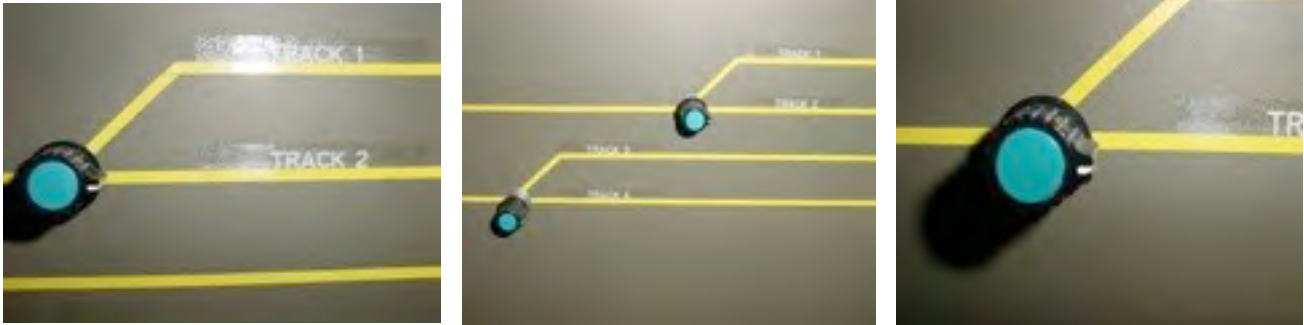
great deal from him. Unfortunately, we never got the chance because, for whatever reasons, Captain Martinell's railroad never made it to the Potomac Division home layout tour circuit. After most everyone else had left that afternoon, a few of us stayed on to discuss the situation, and we sadly concluded that there were probably other such layouts in our area, "ghost railroads" if you will, that are for all practical purposes unknown to anyone but the owners and their families because they are hidden in the shadows of basements and rec rooms.

Several years ago, I wrote an article for the Potomac Flyer extolling the virtues of hosting a division home layout tour, a piece that was inspired by my recollections of the times I had been a tour host myself. I didn't try then (and I won't now) to convince anyone that arranging for a few dozen people to wander around your house for three hours on a weekend afternoon is a good thing, but we are, for the most part, a polite, well-mannered group that is very appreciative of someone who has graciously offered to allow us to come into their

home and see what they've created. Also, we all don't stay for the whole three hours. If you have put the time and effort into building a layout, I hope that this will encourage you to share it with the rest of us, even if you don't believe that your modeling skills reach the bar set by Tory Martinelli or by some of the other superbly done layouts we've been fortunate enough to visit over the years. I have never failed to learn from someone else's endeavors, regardless of the size or format, and I can promise you from my own experiences that at the end of the day you will be amply rewarded with a great sense of personal satisfaction. So bring your handiwork out of the shadows and invite us to come by to see what you've done. That way that we can all enjoy it together.

Tips and Techniques

Point and Go Tortoise Control



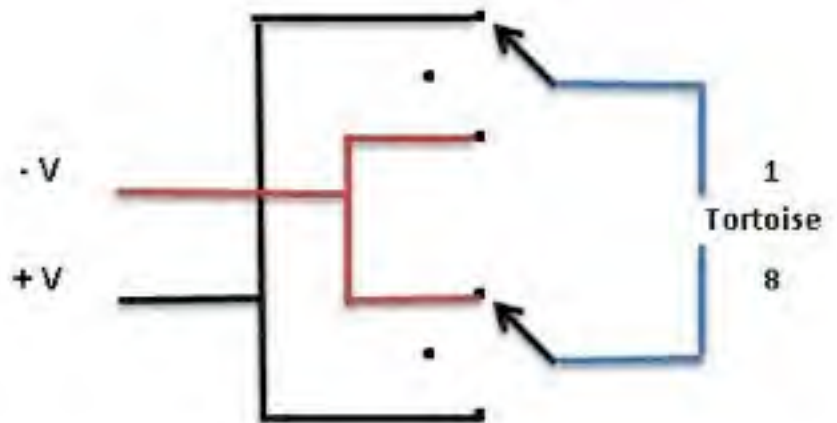
Pete's rotary switch Tortoise controls make it obvious to even the newest crew member how the turnouts are aligned.

PETE LAGUARDIA

I'm a member of an operations group that meets the second Saturday of each month. Most of the layouts are in Northern Virginia or Maryland and employ Time Tables and Train Orders (TT&TO) as their operation scheme. I noticed the preferred methods to control turnouts are to use Double Pole /Double Throws (DPDT) toggle switches to operate Tortoise switch motors or to use Caboose Hobbies Ground Throws for manual turnouts.

I was fortunate enough to be with this group for two years prior to moving to our new home complete with a layout space of

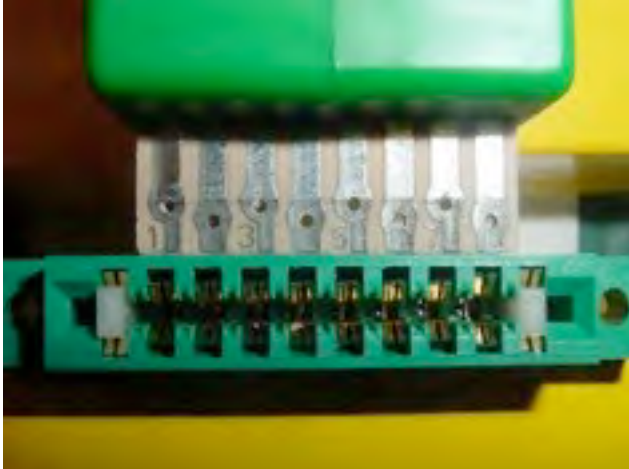
WIRING DIAGRAM



The back of the rotary switch is pre-wired.

Bill of Materials

- Futurlec**
<http://www.futurlec.com/index.shtml>
 POTP4P3: 1 Each 4 Pole 3 Position Rotary Switch
- KNOB12: 1 Each Black Knob and Pointer,
- Digi-Key**
<http://www.digikey.com/>
 EDC305200-ND: 1 Each Tortoise Connector



Using the Digi-Key Tortoise connector (above) avoids the need to solder the leads directly to the Tortoise.

30 x 30 feet. For turnout control, I did not want to use Caboose Ground Throws because they look oversized in the scale environment I want to model. I also didn't want to use toggle switch because their orientation is inconsistent between layouts; on some layouts they are set to move vertically (left and right) and on other layouts horizontally (up and down). Plus, regardless of the vertical or horizontal orientation, the toggle switch did not indicate the direction of the turnout, only how to move the turnout.

My plan was to draw the track plan on my fascia and add the tortoise controls at the turnout locations. I wanted something that an operator can look at the fascia, understand where he is, and know which direction the turnout was facing. I came up



Pete pre-wires the rotary switches and connector at the workbench - MUCH easier than working under the layout!

with using a 4 Pole 3 Position Rotary Switch, with a black Knob and pointer.

By mounting the rotary switch on the fascia with a track plan showing the turnout operators simply rotate the switch to set the direction of the turnout.

I felt this provided a visual cue. The spin off plus to this if you use a tortoise connector you'll be able to do all the wiring on your work bench and just simply hook up two wires to the tortoise bus making installation a snap.

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Achievement News

Brian W. Sheron, MMR



The MER convention was held October 28-30 in Cary, North Carolina. My plan was to attend the convention and thus, this column was expected to be mostly devoted to reporting out what occurred in the contest room.

I was scheduled to ride down there with our Superintendent, Marshall Abrams. Marshall had cataract surgery right before the convention, but expected to be well enough to travel. Unfortunately, about an hour before I was to leave on Thursday to meet him at his house and then drive together to Cary, he called to tell me he had a complication with the surgery and his doctor wanted to see him on Friday. The room reservations were in Marshall's name, and I had not even bothered to find out how to get to Cary, assuming Marshall's GPS would get us there. Thus, after some agonizing, I decided to cancel out as well. I am sure there will be a good description of the convention as well as the contest room, and all of the models that were entered and displayed in the next issue of "The Local."

Not attending was a little disappointing, since if you look at my name under the title, you'll see that I received my Master Model Railroader (MMR) certificate in October. After the banquet on Saturday night, they awarded MMR certificates to several regional members, and I was supposed to have received mine then.

I started working towards my MMR back in 2008. As I usually tell people, back in October of 2008, Marshall Abrams asked me if I wanted to be the Division's Achievement Program Coordinator. My response was "Sure, what's an Achievement Program?"

Well, I went to the NMRA web site and read all I could about the Achievement Program. I found out that one of my duties was to help division members that were working on Achievement certificates. I figured I wasn't going to be a lot of help if I was not really familiar with the program and all of the achievement categories. A long time ago I learned that the best way I could become knowledgeable about something was to participate in it. In other words, jump in and "just do it". What I found was that because I had already built a layout, I had actually met many of the requirements for some of

the achievement categories. For the areas where I hadn't completed the requirements, I figured out what I needed to do, and tried to do it. It was really an interesting and fun experience. For example, it took a little practice, but I learned to scratch-build track components, like a turnout, crossing, and gauntlet track. I had never considered operations in all of the years I actively pursued the model railroading hobby. But, I got involved with an operations group and learned a whole new facet of model railroading I was previously unfamiliar with, which in fact is one of the purposes of the Achievement Program.

I will readily admit that the toughest achievement category for me was "Master Builder-Cars." My first several cars did not get Merit Awards when they were judged the first time. I was really disappointed too. I spent a lot of time on those cars, and at the time, I felt all my work was for naught. But it is sort of like riding a horse. You will probably fall off a few times when you first learn to ride. And, if you don't get right back up on the horse and keep riding, then you'll never learn to ride. What I learned from this experience was "where the bar was." What I thought was great modeling was not that great in the eyes of the judges and did not meet the judging criteria for the cars. Now some of the cars I built could be reworked to address the judge's comments, and I reworked them so that they achieved a merit award to count towards the 4 required Merit Awards needed for this category. Others could not easily be reworked, but they were certainly sufficient to count towards the total of eight cars that need to be built, so all was not lost!

The bottom line to all of this? I don't consider myself to have any inherent modeling skills above those of the average model railroader. What I found was that many of the requirements were not that difficult to accomplish. Some come easier than others, but I think all are readily achievable by the majority of model railroaders.

Division Calendar

January 14, 2012, 1-4 pm

Layout Open House

Roger Sekera, Potomac, MD

January 21, 2012 2-4 pm

White Flag Extra Clinics

Using Structural Steel

Complex Rock Formations Made Easy

Leisure World, Silver Spring, MD

February 25, 2012, 1-4 pm

Layout Open House

Glenn Paulson, Waldorf, MD

March 3, 2012 2-4 pm

White Flag Extra Clinics

Greenberg Shows

The ABCs of Steel Modeling

Reston Regional Library, Reston VA.

March 17, 2012, 1-4 pm

Layout Open House

J.D. Smith, McLean, VA

March 31, 2012


2012 MiniCon

South Columbia Baptist Church, Columbia, MD

July 29-August 4, 2012

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<http://www.gr2012.org/>




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Around the Division – White Flag Clinic Report

BILL DAY

Photos by the author

Making pine trees and dwarf signals headlined the fourth series of White Flag Extra clinics.



Marshall Abrams showed a power point presentation replete with prototype photos, wiring diagrams, trackside photos and specific instructions for making dwarf signals. Marshall uses LEDs smaller than usual— LEDs with both red and green enclosed under the dwarf signal’s hood. The signals show green for mainline use, red for siding use and yellow for trains approaching in the opposite direction.

Dwarf signals are used in yards, before bridges, in passenger terminals and other places where track is tight and sightlines need to be preserved. To be prototypical, dwarfs need to be slightly tilted upward to be seen. Marshall suggests using the signals to show turnout indications, exactly as a prototype line would. The ease and simplicity of making the signals—and their reasonable cost versus manufactured

ones—should appeal to every modeler interested in signaling.

Mike Hart brought an HO module with a variety of trees and structures, then demonstrated the making of several species of pine tree, a construction method so successful he markets his work. Basic materials are dried plant roots, bar-b-q skewers and flocking. He prepares the skewers by painting them with a mixture of caulk, paint and a sprinkling of sand, then drills holes in the skewers, finally inserting contoured ends of roots. He places a tuft of kitchen scrub pad on the top of the pine, spray paints the growth, then sprays the foliage with clear coat— shaking flocking liberally over the branches.

He makes striations in the trunk, washing everything with India ink, leaving a mottled appearance characteristic of southeast pines.

The White Flag Extra was attended by 17 modelers, including Mrs. Hart and one pint-sized junior engineer, at the George Mason regional Library in Annandale. Potomac Division members who could not attend can view the slides of each clinic by turning to the Division website and clicking on “Clinics and Library.” The next White Flag Extra will be held Saturday, January 21, 2012, at Club House Number 2 at Leisure World in Silver Spring, MD.



Around the Division – Layout Tour Reports

Mike White’s Solomons & Patuxent Railway



BILL DAY

Photo by the author

Following the Civil War, American railroads bound the country together in a magnificent network of freight and passenger service. From its first beginnings, represented by TV’s Hell on Wheels, to the computer-driven sophistication of today’s operations, the story of railroads has been movement and operation.

Mike White’s layout represents that prototype movement, joining Tony Koester, Bill Darnaby and Allan McClellan in modeling historic rights-of-way with authentic buildings and industries. Mike has carefully reconstructed Calvert County towns and rail lines in his Solomons & Patuxent Railroad, a line that runs through the Maryland towns of Solomons, Dowell, Lusby, Bertha, St Leonard, Prince Frederick, Sunderland and Owings. The era is 1936 when steam was king and Eastern railroads ran on Consolidations, Pacifics and Hudsons. Stressing authenticity on operations nights, Mike’s crew runs a fast clock that is exactly calibrated to a timetable from the thirties.

Some two dozen modelers and wives saw Mike’s layout in early December, seeing how meticulous modeling and trackwork can add a touch of history

to operations night. Although the railroad is a fictional short line, it follows the right of way of the unfinished Baltimore & Drum Point Railroad. When finished, the S&P will service the Owings passenger station, glide by County landmarks--some abandoned, some still standing-- that dotted Maryland’s countryside. Mike’s signature tobacco barn holds tobacco leaves hanging on tobacco sticks, and his J.C. Lore oyster house will be detailed inside and out. He has perfected what the French call *realisme*.

What’s more, few layout rooms boast the care and craftsmanship seen on the S&P. The benchwork and scenery are the product of eight years’ effort, and they show it. The room is finished, the lighting is bright, the wallboard backdrop is beautifully crafted, the horizon lined with trees and distant hills.

Even the working part of the fascia shows attention to detail. The yard switches are miniature “armstrong” levers, not just positioned on a fascia shelf, but set in bases carefully cut with a scroll saw, the bases boasting coved corners and finished surfaces, cabinet work at its best.

Is Mike pleased with his progress? He should be. One veteran modeler who only operates on exceptional layouts throughout the country, braced Mike and said, “When can I come and operate?”

Around the Division – Layout Tour Reports



Bob Reid's HO Allegheny & Shenandoah Railroad

BRIAN SHERON

Photos by the author

While most of the Potomac Division members live in Northern Virginia, D.C., or in Montgomery County, Maryland, there is a strong contingent that lives in Southern Maryland. Bob Reid is one of those members. And on Saturday, December 3rd, a larger-than-expected contingent drove down to Lusby, Maryland, and saw Bob Reid's magnificent HO-scale Allegheny & Shenandoah RR. As an added bonus, they also got to see the breathtaking view of Mills Creek from the large glass windows on the back of Bob's house.

The A&S is a very large double decked HO railroad with a logging branch the extends upward from the upper level. Radio DCC is used to control the trains over the main line that extends for over 720 feet. It is October 1, 1925. The layout is over 75% scenicked and has over 500 cars and 50 sound equipped steam locomotives. Operations occur on a regular basis usually with a crew of 7-10. The layout depicts a mountain climbing journey from Strasburg, Va. to Elkins, WVa. The A&S is partially owned by the Western Maryland RR so lots of WM equipment is seen on the line. The traffic is mainly

agricultural products such as grain and cattle hauled in 20-30 car trains. Petersburg is the division headquarters and locals are assembled in and depart from the small yard there.

The mythical railroad has branches from Alexandria Va. and Waynesboro, Va. through Strasburg, Va. to Elkins, WVa. Only the western branch from Strasburg is modeled. The scenery is plaster over extruded foam and the benchwork is 37 sheets of 3/4" plywood and 25 sheets of 1/8" masonite. The layout completely fills a 975 square foot basement. The railroad area is full carpeted and skirted and over 35 fluorescent fixtures provide ample lighting. Seven towns are modeled, each with a 25'+ passing siding and multiple industrial

locations. Over 200 structures occupy the layout, many are craftsman kits and kitbashed industrial structures. The impressive mine complex in Dry Fork is over 3' long and features a breaker building and a company town. A 40' ten track staging yard supports the operation.

For those that drove the distance, they were certainly not disappointed!



Upcoming Event: Clinics, January 21, 2011

Don't Miss These White Flag Extras!! Two Clinics in One Day!

Refreshments at all clinics.

Reserve with Bill Day, billday22@verizon.net, or 703/406-4112, although walk-ins are welcome.

WHEN: SATURDAY. JANUARY 21, 2011

WHERE: : Leisure World, 2901 Leisure World Boulevard, Silver Spring, Maryland

Using Structural Steel to Create Industries and Scenes: Tom Brodrick

The structural steel clinic shows use of steel after it leaves the mill and before it becomes a finished structure. Bridges, cranes, shoring, steel construction, and fabrication are covered. This power point presentation uses prototypical photos of the steel industry.

Tom Brodrick, a past board member of the Potomac Division, models an HO Layout with emphasis on operation. He is professionally employed in the iron and steel industry and brings that knowledge to his clinic.

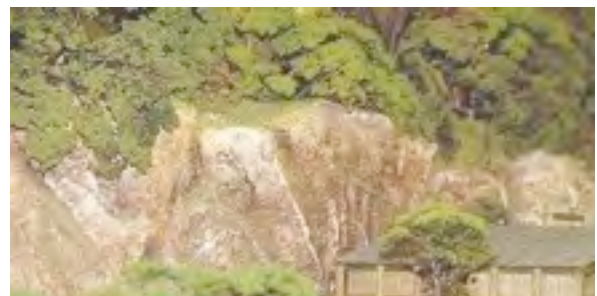


-and -

Complex Rock Formations Made Easy: Bob Johnson

Using a variety of dyes and stains, this clinic shows how to mold, fabricate and assemble complex rock formations in an easy and straightforward way. A major focus is on quick and easy techniques to realistically color assembled castings using a combination of washes, dyes, chlorine bleach and dry color powders.

Bob Johnson, a modeler from the South Mountain Division, has conducted rock formation clinics at Division and Mid-Eastern Region Conventions. His techniques for stippling, filling, carving, dying and highlighting castings produce different looks for different rock strata.



Upcoming Event: Clinics – March 3, 2012

Don't Miss These White Flag Extra Clinics!!

Refreshments at all clinics.

Reserve with Bill Day, billday22@verizon.net, or 703/406-4112, although walk-ins are welcome.

WHEN: SATURDAY, MARCH 3, 2012, 2:00 – 4:00 pm

WHERE: Reston Regional Library, 11925 Bowman Towne Drive, Reston, VA 20190

Greenberg Shows: Bruce Greenberg

Bruce Greenberg, founder of Greenberg Shows and one of the hobby's true champions, will discuss why and how the shows began, the mechanics of their operation, advance publicity and consumer mailings for each show, the recruitment of club and individual layouts and of course dealers. He will illustrate his clinic with power point images of his press releases, press photos and newspaper articles. He will also include photographs of his home layout which traveled the show circuit.

Bruce has conducted clinics in many venues and hosted layout open houses in his home in Alexandria. He draws on his experience as a publisher of collector guides, instruction manuals and historic catalogues, many of them from Lionel Lines. His strong academic background gives him a special perspective on the model railroad industry as seen by both demographers and economists. His entrepreneurial experience in both publishing and train shows provides him with the practical experience in bringing model railroading to a national audience. Don't miss this one!

The ABC of Steel Modeling: John Glaab

John Glaab, the founder of The Magarac Society, a group interested in steel mill modeling, will present a PowerPoint presentation on getting started in modeling a steel mill. Topics will include; how to make the best use of space; choosing your era, adapting non-traditional materials for making unique models. He will use images of his celebrated 4 x 8 steel mill module, Magarac Steel & Iron, showing modeling materials and techniques that can be used to present realistic structures and scenes.

John, a retired NASA electrical engineer and management official, operated Peach Creek Creek Shops for over sixteen years, specializing in brass models and steel mill specialties. He currently runs an Internet business catering to steel mill modelers. He founded and directs the Steel Mill Modeler's Meet held annually, attracting steel mill modelers from across the U.S. and Canada.

John will also discuss his forthcoming book entitled, "The ABC of Steel Mill Modeling."

