

The

June—July 2019

POTOMAC FLYER



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The Potomac Division, Mid-Eastern Region, National Model Railroad Association includes the District of Columbia; Calvert, Charles, Montgomery, Prince George's and St Mary's Counties in Maryland; Arlington, Fairfax, Fauquier, Loudoun, Prince William, and Rappahannock Counties in Virginia, as well as all area independent cities.

Bill of Lading

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Potomac Flyer

Potomac Division's Bi-monthly Newsletter

Submission Deadlines—Issue

Dec 15—February-March	Feb 15—April-May	Apr 15—June-July
June 15—Aug-September	Aug 15—Oct-Nov	Oct 15—Dec-Jan

Cover photo: Gil Fuchs Haifa & Jerusalem Railroad photo by Marshall Abrams.

From the Business Car

by Martin Brechbiel, MMR, Division
Superintendent

I spend a lot of time taking breaks from my editorial jobs and other office work at home. I find it very therapeutic; sitting too long is unhealthy, and also, if I get up and wander down the hall, I can be far more productive in my workshop building models. This is what I do because, “What we do, is what we do; ‘cause all we do, is what we do.”¹ So, my personal interest in model railroading is very heavily focused on model building almost to the exclusion of all else. The very real exception is writing articles about the models that I build.

This is not to say that I have no interest in a layout or other aspects or components of model railroading. I have a layout, yet oddly enough, right now it’s finished, concluded, completed, finalized, accomplished, achieved, fulfilled, discharged, settled, and done, and I am coming to grips with that being somewhat anticlimactic. However, I found that building my layout served its purpose in teaching me what might be possible to achieve. It has also served as a test bed of sorts for scenery and trackwork as well as the placement of structures, including some of those models that I have built and continue to build for articles.

Others in our Division have other interests and are focused on operations, layout design, various modes of electronic control systems, highly precise prototype modeling, modern era, transition era, the 19th century, modules, clubs, or just running trains about the room in whatever scale that their Muse has directed. We as a group are actually quite diverse in interests and modeling activities, yet we all also band and gather together under the NMRA model railroading umbrella. I have no expectations that anyone might choose to do what I do, nor do I feel any compulsion to impose my aspect of model railroading upon anyone. While I may be mystified or even somewhat bemused by some aspects of what others might pursue in their hobby, it’s all good and welcome as I try to learn new things every day. I’ll suggest to all that we just might learn a bit now and again from our Division members.

To that point, we are going to try to set up a clinics program integrated with our open house program (the first of these is announced elsewhere in this issue to take place on Sept 15 at the Northern Virginia Model Railroaders club in Vienna, VA) where we will be able to assemble, talk about model railroading topics, learn a few things, and even socialize a bit as well. (We’re also trying to set up another day of clinics and some open houses on November 16th.) We are trying to

— COMING EVENTS — MARK YOUR CALENDARS

June 22—Layout Tour: Bob Rodriguez’s Nickel City Line

June 29—Potomac Operations Saturday

July 20—Layout Tour: Doug Hess’s Ohio Central RR

Aug 17—Layout Tour: Paul Hutchins’ CSX

Sept 15—New Modeling Outreach Program Inaugural Event

Sept 21—Layout Tour: Mark Gionet’s Boston and Maine Western Route*

Oct 10-14—Liberty Bell Special NMRA/MER Convention

Nov 16—Joint Minicon with James River Division*

* Articles about all these events are in this issue of the Flyer except those marked with an asterisk

1 Lyrics from the song “What We Do” performed by Devo and written by Gerald V. Casale, Mark Allen Mothersbaugh and Max Liederman

avoid conflicts with other local events, but we all realize that every weekend has something else happening somewhere that you might want to attend even when we try planning 6-12 months into the future. However, we really want to develop and present programs that are attractive for our Division's members right here in the Potomac Division. Members who are interested, who want to step up and actively, constructively participate are welcome to contact any of the Board members. Lastly, we're also already looking at the 2020 MiniCon, looking to secure a realistic venue, and then to assemble a new, vibrant program. Again, if you're actively, constructively interesting in participating, please contact any of the Board members now!

Exciting News About the *Potomac Flyer!*

You might have noticed we're calling this the June-July issue of the *Potomac Flyer*, not the Summer issue. That's because, as of this issue, we're going to be published every two months, increasing our output to six issues a year. We're going to do this so that we can all communicate better and more frequently, so that all of our members can be kept better informed of events within and outside of the Potomac Division. As we're looking to initiate a clinics program married to the monthly Open Houses, we need to be able to tell you about these and more, and to do so more frequently, and as early as possible so that you can attend.

That puts more work on the *Flyer* team, for sure, but also on you, our readers. A better way to look at this, however, is that it provides all of you with more opportunity as contributors! So we want and need you to step up and share more articles and photographs, more ideas, and your models with us. This feeds back to the Achievement Program and the Author and Volunteer certificates.

We'd especially like to see some new names as contributors. Don't tell us (or yourselves) that you can't write or take a photo. If needed, we'll work with you to make sure that what gets published not only looks good (well, our *Potomac Flyer* Editor will!), but also helps improve the modeling knowledge and skills of all of the Potomac Division members.

So please, don't hesitate. Contact us at: Potomac-Flyer@potomac-nmra.org with your photos and article proposals. 



Martin is a long-time O scale model railroader who models the South Mountain Branch of the Cumberland Valley Railroad. He also has a strong interest in traction and trolley modeling that freely adopts the Chambersburg, Greencastle & Waynesboro Rwy. He earned his Master Model Railroader (MMR) certificate # 629 in 2019 and is currently the Superintendent of the Potomac Division, Secretary of the Mid-Eastern Region, and also the Editor of *O Scale Trains* magazine. His goal is to make the Potomac Division a thriving environment for all aspects of model railroading while promoting the values of NMRA membership.

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Achievement Program News

by Mat Thompson, MMR

Frank Benenati has earned the Golden Spike Award. Those who attended his Open House in February and those who will be operating on his layout on June 29 can all verify Frank is a talented model railroader.



To earn some Achievement Program certificates, you must document how you built the model

or scene being judged. That can sound demanding, so a natural question is: how much do I have to write?

There is no NMRA-specific requirement, but the answer is: not much. As a matter of fact, AP judges are required to read all the documentation you present, so short documents are appreciated.

Here are some thoughts:

- You aren't writing for a national magazine, so don't agonize over grammar and writing style.
- Judges are looking for the craftsmanship and prototype fidelity of your work, so that's what you want to talk about.
- Instead of narrative, you may find you can make your points just fine using bullets (like you are reading right now).
- Photographs are great storytellers, and smartphone photos are just fine—actually, they are pretty good these days.
- Use your explanation to highlight your craftsmanship and scratchbuilding—if you made tin roofing by cutting up a tin can, make sure you tell the judges.
- Explain unusual techniques and/or materials—don't assume the judges know what work is required to do things like making your own decals or preparing 3D drawings.
- Don't bother with the mundane—"I glued the roof to the walls"—unless there is something special you are trying to point out.
- Include prototype photos if you have them.

Potomac Operations Saturday — June 29, 2019

Sign-up cutoff: June 5

We still have a few spaces for the Potomac Operations Saturday on Saturday, June 29, 2019. Pick a railroad and send a message to ocrr@comcast.net to get on a crew. The final day for sign-up is June 5.

You are welcome if you have never operated, have a little experience, or are an old hand.

We will offer two layouts in Maryland and two in Virginia and will have two operating periods that day—one from 9 to 12 and one from 2 to 5.

Sorry, all the layouts are in basements and none are handicapped accessible.

Here are the layouts:

9 to 12: Pete LaGuardia,
Haymarket, VA:
HO scale New York Central Western Illinois Division. He uses NCE DCC, and TT&TO operations.



9 to 12: Frank Benenati,
Damascus, MD :
HO scale Maryland Junction interchanges with N&W, PRR, B&O, and WM. Frank uses Digitrax DCC, car cards, a timetable, and a fast clock to simulate operations.



**2 to 5: Bryan
Kidd, Nokesville,
VA:**

Alleghany on
the Chesapeake
& Ohio Railroad
using Digitrax
DCC.



**2 to 5: Fritz
Dahlin, Columbia,
MD:**

The Broken
Timber
Subdivision
represents a
fictional Chessie
System line in
central Maryland.
Control is NCE
DCC. Car cards
and simplified
train orders are
used. ☒



The Potomac Division's New Modeling Outreach Program

When: September 15, 2 - 4 PM

Where: Northern Virginia Model Railroaders club, 231 Dominion Road NE, Vienna, VA

Revised Accessibility information: The NVMR Club is NOT handicap accessible. While the waiting room, where the clinic will be held, is accessible, the bathroom and layout are not. The bathroom is on the same level as the waiting room but the door is not mechanical nor are there grab bars or other modifications to assist visitors. To view the layout a visitor must climb three steps and certain parts of the layout are only accessible by steep steps.

The Potomac Division is pleased to announce a new program of educational and information-sharing activities. The inaugural event will be on September 15 between 2 and 4 PM at the Northern Virginia Model Railroaders club at 231 Dominion Road NE in Vienna, VA. One of the core features will be to have the host of a past or upcoming open house provide context and insight into building his or her layout, including history, modeling techniques, and the problem-solving methods used to overcome the innumerable challenges we all face.

The September program will feature a short meet-and-greet with the new Board of Directors, followed by Mark Gionet discussing his Boston & Maine layout in HO scale, which will be open on the following Saturday (Sept. 21).

Our second presenter is a well-known figure in the modeling world, Paul Dolkos, who models Baltimore railroad operations. Paul tells the Flyer he will present a program about modeling tracks in the street. He will describe his layout and then some of the construction techniques used to create former prototype scenes that existed for many years on Pratt St., Fells Point, and other streets in the city.

Our final clinician will be Zach Pabis, a teenage member of the NVMR club,

who will discuss the increasingly popular art of 3D Printing. If time permits, attendees will have an opportunity to ask questions of our Master Model Railroaders who will be in attendance.

The Potomac Division leadership is dedicated to offering “the best type of programming that the members wish to support.” As Andrew Dodge, MMR and Senior Assistant Superintendent says, “We have an untapped powerhouse of first-class modelers waiting to come out and share their time and





talent. I am anticipating a strong show of support from the members so we can mount the highest quality clinics and demonstration workshops possible. I hope our Potomac Division community wants to join us in what should be an exciting future.”

So, join us for an afternoon of model railroading activities designed for beginners and experts alike. Please mark your calendars now for the September 15 event! ☒

[Return to Bill of Lading](#)



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Liberty Bell 2019 Update

Liberty Bell 2019 is fast approaching. We've been busy on your behalf, readying a memorable fall get-together. The clinic schedule is filled, 41 layout owners have agreed to sponsor open houses, and several operating sessions have been scheduled. There is still room for layout owners outside the Philly division who want to open their layouts for travelers to let us know their intentions. Those interested should email Bill Fagan at wfagan@comcast.net. There is also ops information available on the convention website at www.libertybellspecial.org. Note, you can sign up for automatic updates on the convention status on the site.

In our last communication, we described the extensive, behind-the-scenes Saturday trip available on the former Reading branch now called the Colebrookdale Railroad. This all-day trip will demonstrate everything from locomotive startup through duties of the various crew members and will include opportunities for photo run-bys. Sign-ups for this trip are on the convention website at libertybellspecial.org. Please note, there is a capacity limitation for this trip, so register early.

Remember that the host hotel has extended their room rate discount to attendees due to the Columbus Day holiday on the Monday after the close of the convention. With this in mind, there are a couple of NON-NMRA SPONSORED/NON-NMRA ENDORSED attractions available to attendees after our wrap-up Sunday morning. The historic Norfolk & Western (Norfolk Southern) steam engine #611 will be offering trips under steam Sunday and Monday on the Strasburg Railroad. We have reserved a block of discounted tickets for the Sunday afternoon trip which are available through the convention registration page. Tickets are \$10 for adults, \$6 for children. Please note, this reservation must be made by July 31, 2019 as the remaining tickets must be returned to the railroad on August 1st. Strasburg is approximately 65 miles west of the convention hotel. Also situated at Strasburg is the Railroad Museum of Pennsylvania, dedicated primarily to the Pennsylvania Railroad, but of major interest to all train enthusiasts. More information on either of these attractions can be found at www.strasburgrailroad.com and www.rrmuseumpa.org. In a different direction, weekend operations are scheduled on the tourist railroads Reading and Northern Railroad and the Lehigh Valley Gorge Railroads, which are a little over an hour's drive northwest.

As you can see, we have a lot planned at the convention and there are many opportunities to have a toal railroad experience while you are in Pennsylvania. As we said before, all roads lead to King of Prussia! 

[Photos for the Flyer Wanted](#)

While we are delighted to get your articles on the various aspects of model railroading, we would also love to get your photographs. Do you have a shot from your layout you feel like sharing with us? Maybe it's a prized locomotive or a structure or scenery or an example of weathering. We'd be delighted to have you send it to us for possible publication. It might even become a cover photo! JPG, GIF, or PNG formats are best for pictures. Send them to: Potomac-Flyer@potomac-nmra.org

Moving: Dismantling A Layout Then Getting It Up and Operating Quickly

by William Lyders

Almost all of the articles in model railroading literature about moving a layout involve carefully preserving the assets you may want to keep, discarding quite a bit of the old layout, designing a new layout, and spending years making a new layout. But that is quite a bit different than what I wanted for my layout move.

I wanted my new layout to reuse most of the old layout's assets and to become operational within a year. This article will tell you the measures I took and the lessons that can be learned by my experience in the move. I had my new layout operational in six months and have reused most of the previous layout—even though it wasn't built in sections or originally designed to be moved.

Assessing my original layout assets

I had been working on my dream layout for over 25 years and had recently conducted three operating sessions on it, so it was a mature layout with a lot of operational assets. And



Figure 1 - Layout removal complexity for the 2-level W&V in one room and a 2nd level portions in another room.

now I wanted to move to my retirement home and take my layout with me. Literature told me that I should plan on moving all the structures, rolling stock, locomotives, etc. and trashing the benchwork, track, and scenery. But at my stage in retirement, I had a better idea: I knew I could carefully take apart my current 440 square foot double-deck Whitewater & Virginia (W&V) in an advanced state of construction and pack it up for reuse in my new retirement house basement, including many dioramas or mini-scenes that I wanted to keep, as shown in Figure 1.

So, how does one begin to proceed in a situation like that? The obvious answer is to move the layout in sections and set it up the same way in the new basement. But like most of us, I didn't build my layout in "sections." I just kept building benchwork and sub-roadbed to meet my layout mission statement and operational plans.

Carefully taking down the old layout

So a month before the closing on my old house, I methodically started to take apart my old layout and store the pieces in my garage and open areas of my basement:

- I acquired three large boxes from a neighbor who was just moving into my neighborhood (we got many large boxes from them for this layout move) and placed all of my completed structures in them, along with lots of foam shipping beans.
- I placed all my completed rolling stock and locomotives in my white commercial carry-on train boxes.
- I used wardrobe boxes to store all my plaster scenery pieces, including a 10-foot cliff and river along a rail yard. The movers did move these heavier boxes for me.
- The second-level benchwork in the old layout was secured to the wall by large screws (sometimes a challenge to remove) and some 2"x2" supports. I kept all the benchwork and the ½" plywood/Homasote subroadbed with track on it.
- My "sections" were different construction types (L-girder and open grid) and with various sizes, mostly 16"x96", 30"x60", up to a 5'x9' section (with my turntable, roundhouse, and feeder tracks on it). All of the sections made the move in good condition, except one L-girder section that had a collection of curved tracks that crisscrossed each other—really a subroadbed mess.
- All of the track was retained on each of the "sections," including the wiring (power bus and track feeders). This way, all I would need to do was connect the power busses together in the new layout power grid.
- I retained my power control panel with my Digitrax DCS100 and DB150 and my power district circuit breakers and RRampMeters installed.
- I did have a helix (50" diameter x 24" high) that I moved but later donated to a member of our model railroad club.

Moving the layout

The old layout pieces had been staged in the basement in boxes with the longer pieces stacked in the garage ready for the move:



Figure 2 - The moved layout assets were organized and placed in logical construction piles in the new basement.

- I rented a 15-foot U-Haul truck and, with the help of three friends at my old house and three other friends at my new house, we packed up all the old layout pieces from the garage and many boxes of collected layout material and boxes of rolling stock/locomotives/tools/scenery material/paints, etc. and filled the truck. Both houses had walk-out basement doors, so transporting layout stuff between them was somewhat simplified.
- The layout material was arranged in logical “piles” in the new basement, as shown in Figure 2, so that I could begin construction quickly on the new layout.
- I did take all my 36”-38” length 2”x2” legs from the lower section of my old layout (packed in a box) but donated them to another club member when I decided to have 48” legs on my new layout.

Reuse of assets for a quick construction on the new W&V layout Construction of the new W&V began by considering two firm conditions imposed by my wife: No holes in the walls— I.e., standalone sections No painting of the walls in sky blue—I.e., use backdrops on sections These conditions were imposed because my wife saw the extensive effort I had made taking down my old layout and restoring the walls in the old layout room. She didn’t want us to

face that task again should my health (which today is very good) deteriorate. I had read about this take down/restoration effort in Marshall Abrams' *Estate Planning for Model Railroaders*, now available on the Potomac Division website Special Interest Reports page at http://potomac-nmra.org/Special_Reports/index.html.

Construction began by standing up all the sections on their new legs and then assembling them in a logical order in accordance with my new layout plan sketch:

- The new W&V plan has 22 sections comprising two peninsulas and a long stretch along the back and side wall.
 - I was able to keep my old W&V Mission Statement and Operational Concept.
 - Operations on the W&V center on the town of Whitewater (i.e. Lynchburg, VA where the N&W, C&O, VGN, and SOU all go thru or nearby) with runs from WVA to Tidewater and NC to Alexandria (NVA). Eventually I kept/expanded the four staging yard sections for these locations.
 - The sections were assembled so that they roughly connect to the original W&V sections.
- The 22 sections included 18 reused “sections” and four new sections: two new corner sections (each about 3’x3’), one new section (16”x96”) for constructing my NMRA Engineer-Civil AP certificate trackwork and industries, and a new section required to provide lead tracks into my old Whitewater Servicing Facility (WSF) with the roundhouse/turntable on it.
- My only purchases to date are 96 new 2”x2”x48” legs and some Masonite sheets cut into 16”x96” pieces for backdrops—all cut accurately for me at Lowes.
- With the assembled sections standing in place in the basement, construction involved systematically connecting the new sections together to align the reused track. Each section was screwed together with the connection points clearly marked with magic markers at the connecting screws. This will make it easier to disassemble when that time comes.
- Section assembly priority was given for sections required to launch trains on the continuous run around the two peninsulas and the peninsula connecting section when power could be applied. This was achieved in six months when those 15 sections were assembled. Progress on the remaining sections is proceeding smoothly.
- Shelving was constructed beneath some sections for access to stored assets and construction supplies.
- Power to the 15 sections was achieved by connecting the respective section power busses together. Power was run from the DCS100 or DB150 to the power districts via power busses. The power busses for each section were connected as that section was assembled.
- Reconnecting track was a little challenging, as a Dremel cutting tool was used to cut the track on the original W&V, and sometimes the gaps remaining had to be dealt with. But with lots of reused track available, adding small pieces of track when needed was not difficult.
- I did install many reused scenic features (a long trestle, the Thunder Gorge diorama with bridge, a long, curved bridge over a lake, the Bear Creek curved trestle, the roundhouse/turntable and associated tracks) in similar settings, but not necessarily in the same places as the original W&V.

- Rock formations, cliffs, and forests were installed on their original sections or occasionally moved to the new corner sections.
- Lakes and rivers were mostly retained but modified somewhat for a better viewing location.
- I reused the middle section from the original W&V above the town of Whitewater but repurposed it from a working yard to an industrial park. This allowed me to reuse some scenic bridges above, and then construct a new 2.5"x96" girder bridge to climb to them.
- The new Masonite backdrops were all painted a sky blue and installed on the back of the sections. One peninsula has a scenery divide of backdrops on it.

The DCC Loconet cabling was reusable, as it only had to be run from the DCS100 and DB150 out to the UP-3/5s (Digitrax Universal Panels for plugging in throttles) on the fascia. Of course, the fascia was in different locations, as were the sections from the original W&V. But setting up the W&V Loconet was not difficult.

- The fascia/skirting brought up some decisions to make. With the layout using 48" legs vs. the old 36" legs, and Sections from the original W&V upper level only requiring 10-12" fascia, I now had to decide (a) if I wanted to make new taller fascia boards and

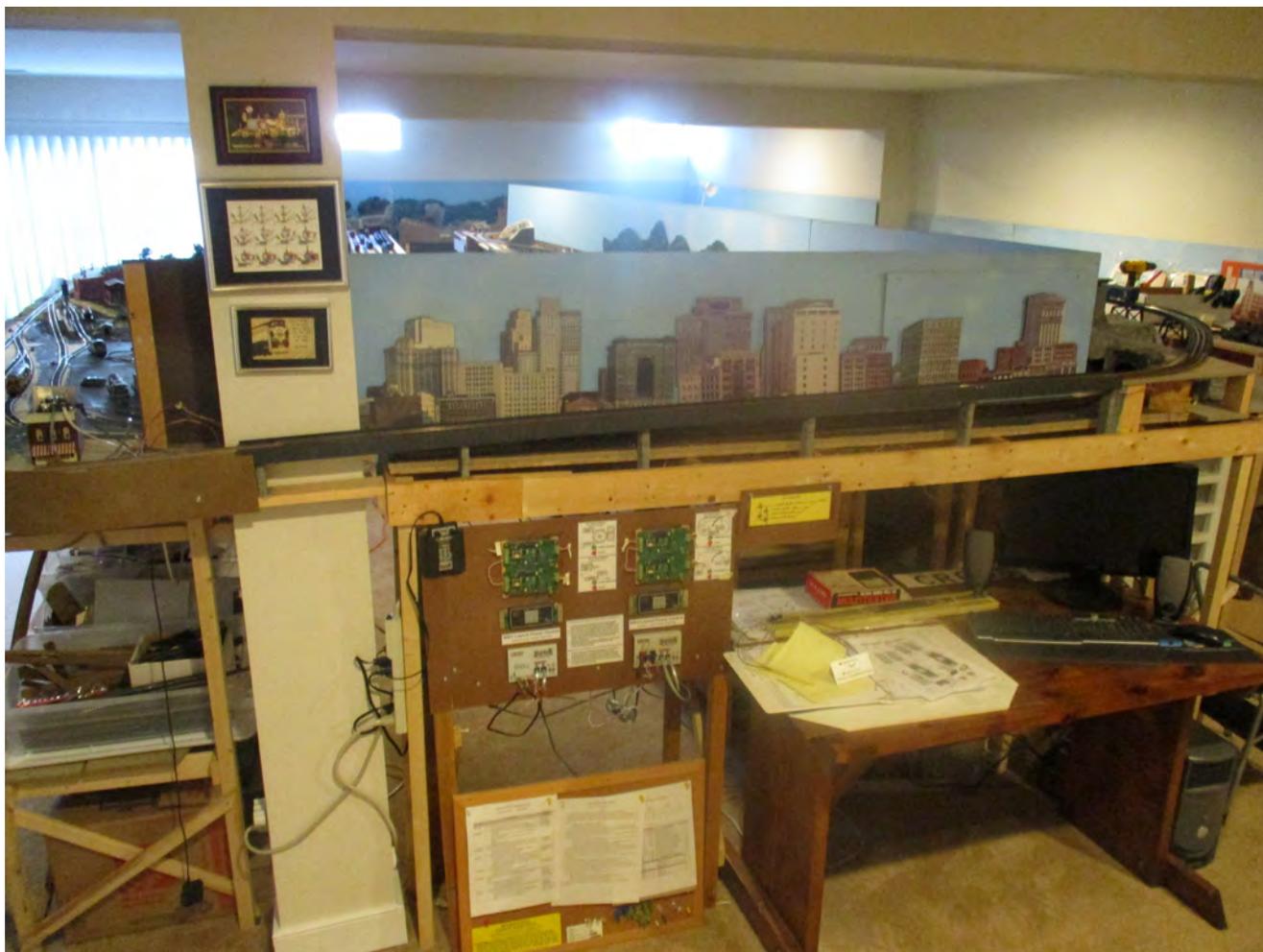


Figure 3 - The initial view of the new W&V entering the room shows Dispatcher and many sections connected

retain the original skirting or (b) retain the original fascia boards and get new taller skirting. I initially mounted the original fascia boards where UP-3/5s had been mounted so I could get the Loconet operational and start running trains. But in hindsight, I realized I will have to make new fascia boards for over 30% of the sections and taller fascia boards would look better in some of the Sections anyway. So I have chosen option (a).

Viewing the new W&V

A visitor entering the new W&V from the stairs down to the basement will see an approaching helicopter view of the whole layout until he or she gets to the bottom of the stairs, as shown in Figure 3. This view also shows the Dispatcher station, Northern VA Yard, and the power control panel.



Figure 4 - View from the crew lounge to the W&V Servicing Facility and the working middle aisle [Sections](#)

The two main views once in the room are:

- 1) Looking from the new crew lounge towards the Whitewater Servicing Facility (WSF) and the working yards in the center aisle with the NC Yard, the Interchange Yard, and the Meat Packing Plant & Ice Platform, shown in Figure 4, and

- 2) Looking down the main aisle with the Eagle Lake Bridge and bridge over the town of Whitewater on the left and on the right, the Tidewater yard, a horse ranch, and the under-construction Civil AP Section, down to the end of the room where the D&D Mines are located, as shown in Figure 5.



Figure 5 - The view along a main aisle with long bridges, the main town, a staging yard, and mines.

Unexpected challenges in the construction Not all the new construction went smoothly. But no problems were encountered that could not be resolved: One section “fell apart” during the move. But it did provide many 2”x4” risers and power bus wiring for reuse. Also three bridges, an industrial park, and a river from that section were reused, but at new locations on the new W&V The power control panel was completely reused. However, connecting the Loconet cables and power cables from it to the new W&V provided a challenge. I had “documented” this panel in my AP electrical certificate documentation, but didn’t make some of the cable connections clear enough. I had no trouble getting the DCS100 and two of my four power districts up and running. However, a few weeks before I tried to bring my DB

150 and its two power districts and Loconet UP-3/5s operational, I made the mistake of running an analog locomotive on my DCC layout. Some really screwy voltages (DCC expanded from 14V to 23V and a DC component later) occurred. I had to draw upon the Digitrax Yahoo Group and a DCC friend to identify the problem and then clear the analog loco from my active list so the DCS100 generated the correct voltages. My DCC friend straightened me out on how to run a Loconet from both the DCS100 and DB150 and then hook my DT100 to an UP-3 to bring the system up correctly. My documentation needs updating now.

- The D&D Mines/Power Plant section required a new main line as the previous tracks to this had entered at a 90-degree angle and now needed to go directly across the section. This new track required modifying the background mountain scenery with hidden track. It was actually the only section moved totally intact as it had shelving included too. (See middle rear of Figure 2).

The staging yards in the original W&V were jammed in a closet and entered the layout thru holes in walls. Now they were out in the open and had to be located so that operations as described above could still be done. This was easy except for the Appalachian Yard which had to now be placed in the middle of the room. This will mean a longer around-the-layout run to the Tidewater and Northern VA (NVA) staging yards.

Support for Estate Planning Regarding the New W&V I had a very detailed W&V Inventory of the assets of the layout and collection (whether built or still in boxes to be built). I had worksheets in the inventory (Excel) spreadsheet for Locomotives, Structures, Rolling Stock, Electrical, Tools, Scenery, Scene Details, Figures, Vehicles, Tools, etc. I capture product info, asset catalog #, car/loco number, DCC loco #, purchase date/price, and action needed (build, built, update).

However, as I stated earlier, I took apart the original W&V and created 22 sections. So I needed to add a page in my W&V Inventory to capture these new section assets. In Marshall Abrams' *Estate Planning Special Report*, a discussion of assigning a market value to layout

W&V Layout Sections						3/24/19
#	Name	Description	Size (WxL)	Unique Feature	Market Value	Action Needed
1	W&V Servicing Facility (WSF)	Roundhouse, operating Turntable,[14 Tracks], Steam and Deisel Loco servicing Tracks; mainline around the WSF	5'x9'	2 Shelves built into benchwork		
2	WSF Transition Leads	6 Entry tracks from mainline into/exiting WSF; Industry and loco cleaning track	5'x4'	1 shelf built into benchwork		
3	Meat Packing/Ice House	Double track mainline with double crossover; Industry siding with Meat Packing and Ice House for Reefers	16"x8'	Power Panel and Dispatcher staion under Section		
20	Horse Ranch	Old horse ranch module with house, barn, pond, and team track	24"x48"	Open storage underneath		
21	Local Coal Distributor/Industrial Park	Track for delivering coal to truck industry; An industry; 2 bridges	24"x30"	Open storage underneath		
22	Tidewater Staging Yard	5 tracks with lead-in mainline; 2 industries and background industrial buildings	24"x95"	Open storage underneath		

Figure 6 - Sample new section page in the W&V inventory spreadsheet

assets was recommended for your executor or the person charged with settling the estate and removing the layout. This section page will have info on all 22 sections similar to that shown in Figure 6. Hence my Estate Planning actions, per the Special Report and summarized in Marshall's January 2019 NMRA Magazine article, Page 34-35, is to add the section page and include a market value column for everything in the inventory. I will also include modify the column for where an asset is located on the W&V now to the layout section.

Conclusion

Whether you are in one of the model railroad clubs that have lost or are losing your location, or just a model railroader who wants to move and retain a large portion of your layout in order to have a working railroad, I have demonstrated it can be done. From an estate planning viewpoint, if you want to sell or donate whole sections (and sell the on-board assets) for someone else to use, this approach will work also. 



Bill Lyders is a retired defense systems engineer. His W&V layout is his third layout over the last 45 years. The first two were bedroom-sized layouts and were “moved” in the old-fashioned way—save some stuff, trash most, and do a new design and build. But he joined a local model railroad club, the Prince William Model Railroad Club (PWMRC), in 1995. Bill says his skills grew and his layout grew until he was going through walls, making a double-level layout into two rooms. Bill is now working on his NMRA AP certificates.

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Dress Up Your Layout

Scratchbuilding Telephone Poles

by Nicholas Kalis

Most model railroaders can probably find some use for telephone poles on their layouts. If you haven't dived into scratchbuilding yet, this can be a fine introductory project. Many of the materials can be found in your local hardware store, though the ivy will likely require a visit to the Scenic Express web site to order.



The prototype for my Fn3 (1:20.3) models of wooden telephone poles with light fixtures appear in several photographs of the Waipahu Engine House in Waipahu, Oahu (Hawaii). Prototype light fixtures were surely meant to make evening working conditions safer for engine house employees.

Here are step-by-step directions for making your own telephone poles in any scale. Be aware that you will need to find light fixtures in your scale. This should prove quite easy. I used products from Loco-Boose Hobbies because of the unusual scale I model in (at least unusual for an indoor layout).

- Each wooden dowel was cut to length with a Microlux table saw (extra length was provided for to set each pole deep into the pink foam baseboard) but probably any hand saw will do.
- All dowels were scraped with a miniature saw to impart grain to each pole.
- Each dowel was stained and left to dry (use a stain that suits you or work from prototype photos).
- I built cross pieces of strip bass wood,
 - ◆ cutting, staining, attaching them with hot glue gun.
 - ◆ (I used an X-Acto® X3254 Craft knife—Black to cut a flat spot for better adhesion.)
- I built two diagonal supports for each cross piece by cutting strip basswood,
 - ◆ staining, and installing them with hot glue
- I cut off a piece of Miniatur Ivy,
 - ◆ stretched the ivy and glued on to each pole using Elmer's White Glue.



- Each Loco-Boose Hobbies light fixture was shortened by cutting the tube with an IVY Classic 19050 tube cutter,
 - ◆ bent each tube section straight,
 - ◆ weathered by dipping into Bragdon Enterprises rust weathering powder,
 - ◆ sprayed each lamp with Testor's Dullcote,
 - ◆ used a paper towel to wipe off excess powder,
 - ◆ allowed fixtures to dry.
- I attached weathered lamps using hot glue.
- To simulate four insulators, plastic beads were attached to each of most poles.



Tube Cutter

- ◆ Each plastic bead was sanded on one end by placing the sandpaper on a flat surface and rubbing the bead back and forth on said sandpaper.
- ◆ coated each bead with Testor's Dullcote to remove sheen,
- ◆ left them to dry,
- ◆ then attached them to the cross arms with superglue.
- Some green paint is painted at the base of each dowel to simulate mold growing on the telephone pole in this somewhat tropical climate of the prototype.
- The last step can be to attach a model pigeon to the cross arm using a model in the scale you have chosen. In some cases, the pigeon must first be painted. Painted pigeons are a short-cut. I used a Preiser 47084 Pigeon.

There you have it: scratchbuilt telephone poles that will not look like anyone else's.

Commercial Components

1. Preiser 47084 Pigeon 1:25 scale
2. Mininatur 936-325 English Ivy O scale
3. Loco-Boose Hobbies #DBL 10 Depot Lamps w/o light bulbs G Scale

Materials Used

- | | |
|--|--|
| 1. Manufacturer Unknown Dowels, wooden | 8. Custom made stain |
| 2. Manufacturer Unknown Strip bass wood | 9. Manufacturer Unknown Beads, green |
| 3. X-Acto X3254 Craft knife - Black | 10. Manufacturer Unknown WT-260 Glue gun |
| 4. X-Acto No. 11 blades | 11. Manufacturer Unknown Glue Sticks |
| 5. Testors 1260 Dullcote Clear Flat Lacquer Overcoat | 12. Manufacturer Unknown Paper towels |
| 6. Helping Hand Aluminum Oxide Sandpaper | 13. Elmer's White Glue |
| 7. Bragdon Enterprises Weather System Medium Rust | 14. Bob Smith Industries Maxi-Cure (CAA) |
| | 15. Ivy Classic Tube Cutter 19050 |
| | 16. Miniature saw |

[Return to Bill of Lading](#)



Nick Kalis is Potomac Division Clerk. He writes: "My love of model trains came from seeing them on television. That led to the proverbial Lionel set under the Christmas Tree at about the age of eight or nine. My adult layout experience has been a never-completed HO Sunnyside Yard, then on to the Lower Montauk Branch LIRR featured on the cover of RMC. My current Oahu Sugar Company in Fn3 graced the cover of *Narrow Gauge Downunder* and was the feature story of the July 2018 issue."

Dress Up Your Layout

Making A Backdrop Transition

by Alex Belida

After adding the backdrop to my layout (see the Spring 2019 *Flyer*), although pleased with the effect, I felt I needed to add something to ease the transition between layout and wall. So I came up with the idea of making what I call "rocky flats"—when building flats won't do.

I took some spare one-inch thick, two-foot square pink foam insulation, traced the outlines of low hills and cut out the parts I needed.

I then did some sanding and shaping, carving and cutting, and mixed up a batch of Sculptamold and spread it roughly over the foam.

After letting it dry, I went over the entire piece to sand off any unnatural looking bits of the plaster. I also did some further carving to create fissures.

Then it was time for my standard terrain base paint: a mix of yellow ochre with a little dark umber added.

When I was satisfied with the overall look, I went over the entire flat with the diluted India ink mix I use on just about everything.

After checking the position to ensure it fit snugly between the layout and the wall, I glued it, using foam tack nails to hold it in place while the adhesive dried. I then added some scrub brush and rocks and bits of grass clinging in the cracks, using basic white glue to hold these where I set them down.

I made a total of three of these "rocky flats." I placed them so they partially overlapped with the mountains I had spray painted on the backdrop.



One of the unexpected bonuses was that the shadows generated by the flats created what could be viewed as another line of distant mountains on the backdrop.

(The foam squares came from Home Depot and are relatively inexpensive at \$5.98 per piece. The Sculptamold is from a bag I've been carrying around for more than 20 years. And yes, it's still good. The acrylic paints come in large tubes I picked up from a nearby Michael's. The ground cover is mainly from Woodland Scenics. The small boulders were leftover bits of Sculptamold rolled into shape and then dipped in my base terrain color and left to dry.)

[Return to Bill of Lading](#)



Alex Belida is a retired foreign correspondent and news executive who worked for the Voice of America. Besides living in Europe and Africa, he traveled extensively in the U.S. and abroad while covering the White House and the Pentagon. He and his wife live in Rockville. His Eureka and South Pass RR is in a second floor 10' x 12' bedroom. It will be on the layout tour list for 2020.

Book Review

Lance Mindheim's *Model Railroading As Art*

by Mat Thompson, MMR

Seldom can a book review in our hobby mention the Mona Lisa, but seldom do we see a book as creative and thought-provoking as Lance Mindheim's *Model Railroading As Art*.

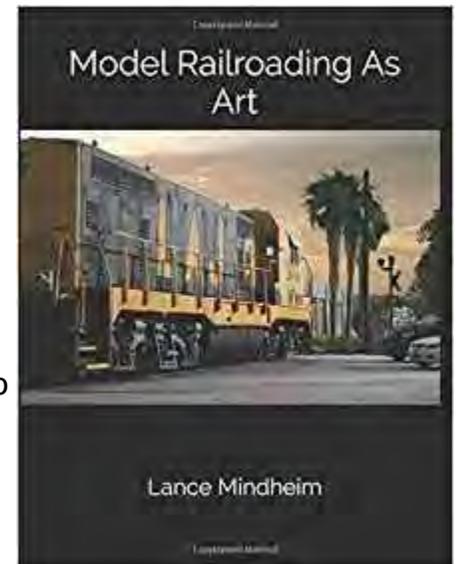


If Leonardo Da Vinci had painted a crowd in the Mona Lisa, the enigmatic curve of the lady's lips might have barely been noticed. And, if the colors and shapes of the background weren't muted by her hair and her dress, our eyes wouldn't have been immediately drawn to her face.

Da Vinci so carefully composed the portrait that even if you take just the slightest glance you will find yourself looking at her eyes. Go ahead, peek at her and see if I am right.

While Lance doesn't torture the Mona Lisa as I have, he sees the careful composition of great art as a technique to guide us as we build scenes on our railroads. Structures and scenery can set a mood and tell the story rather than becoming a visual clutter of models, regardless of how well each may be built. In Lance's words "We can enjoy our work as a standalone piece of art, something we enjoy looking at, whether it's in motion or not."

As Lance explains concepts, like Unity and Variety, and Balance and Emphasis, and Focal Point, study his photographs. This one from page 79 is a good example of the principles of design applied to his modeling.



Your eye goes first to the engine. All the other elements in the photo—the buildings, the trailer, the bare parking lot—belong and complement the scene. But just as the Mona Lisa is a portrait of a woman, this is a scene of a switch engine at work. The scene is composed to tell that story.

Notice how the empty parking lot helps to set the mood—just another working day in a run-down industrial part of town. More structures and details would distract the eye and confuse the viewer. Lance calls vacant lots, parking lots, forests, and fields negative space we can use to create visual balance. As he explains the idea with photos of several dramatic, yet mundane scenes, the modeling takes on an uncommon level of realism.



The book is also filled with Lance's practical advice. One example: his guidelines for compressing scenes. He suggests streets and roads shouldn't be compressed at all but the length of city blocks and spacing between telephone poles can be reduced by a third and still retain a look of realistic proportions. He has similar advice on structure compression and compressing scenes from the front to the back of a layout.

Lance also talks about colors, color tones and weathering effects. Well-illustrated with photos, he shows both useful products and effective techniques. He begins with good observation and then careful thought about the desired effect. Only then does he proceed to the actual modeling. The reward is captivating scenes.

If you are an armchair modeler, *Model Railroading As Art* will keep you entertained for hours. If you are a serious modeler willing to improve your skills, your copy will soon be tattered, book-marked, and paint smudged like mine. I suspect that would delight Lance.

Model Railroading As Art is in stock at Mainline Hobby. You can also get the book from Amazon. The price is \$29.95. 

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Mark Me Up: A Visit to Austin

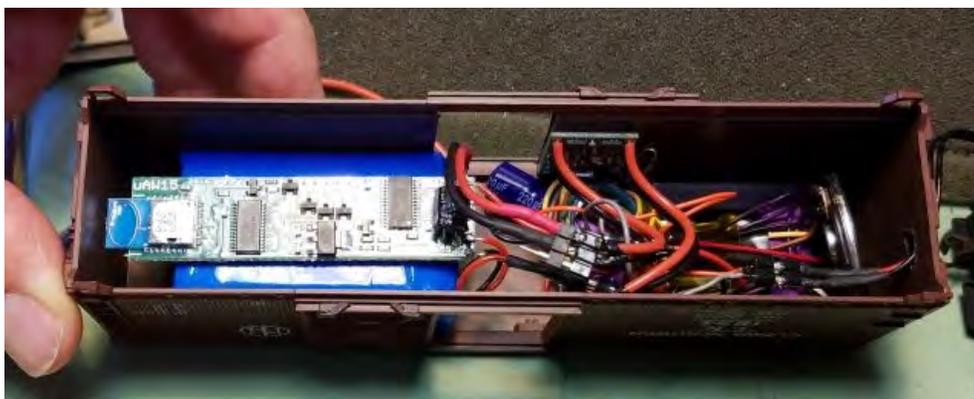
by Mat Thompson, MMR

Recently I visited Riley Triggs in Austin, Texas, and operated on his HO scale Port of New York/Hoboken Shores railroad.

Riley's layout depicts the pocket yards in New York City and the busy New Jersey waterfront where car float docks were used to move freight cars in and out of Manhattan. He has created a switching-intensive environment that is both true to the prototype and fully capable of supporting operations.

It's a unique prototype, and Riley's approach highlights several ideas layout owners and operators might want to consider.

Battery powered HO layout: There is no wiring on the layout at all—none. Each locomotive has an AirWire receiver and a battery. Each engine's DCC decoder gets power and instructions from the AirWire receiver. A box car holds the battery and DCC decoder since they don't fit in an HO diesel



The box car holds the 9 volt battery, AirWire receiver, and DCC decoder. Engines can run more than 3 hours on a charged battery.

switch engine. Engineers use an Easy DCC throttle to control their trains. Other than not trying to set out the battery box car, the fact that the layout is battery powered is immaterial for operations.

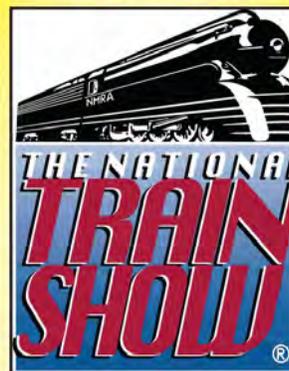
No scenery: Presently the layout has no scenery. Track is glued down on insulation board. Operations are possible because Riley has labeled all the tracks and locations. For an engineer or conductor, it is very easy to determine where cars are to be picked up or set out.

Simple operations: Riley's layout is all about switching between



yards, industries and car floats. The only paperwork is switch lists. There is no schedule, no car cards, no dispatcher and no trains going from one place to another.

You're the kid.



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Brakeman Joe: Joe is a figure used to represent a brakeman. Normally he is hanging on a car or standing on the switcher pilot while the engineer runs the train to the next location that needs a brakeman—either a turnout that needs throwing or a car that needs to be coupled or uncoupled. Once there, he is put on the ground so he can do his work. The idea is these things can't happen without a brakeman to do them. Moving the brakeman to the spot where he is needed adds realism and slows down the pace of operations.



Brakeman Joe is hanging on a boxcar while the engineer moves the train to the next spot where Joe will throw a turnout or couple a car.

So there you have it—a layout with no scenery, no power in the rails, and virtually no paperwork, and plenty of operating challenges! ☒

Flyer Contributions Wanted: Here's What To Do

We at the Flyer want to encourage submissions from Potomac Division members. We know you have experiences and knowledge worth sharing with your fellow modelers. If you have an idea, please send us a brief email description of what you propose to write. Send it to: Potomac-Flyer@potomac-nmra.org

When you get approval, please observe the following steps to get your contribution into print.

First, compose and submit your articles in one of the following preferred formats: TXT, DOC, DOCX, or RTF. Please do not double space after periods.

Then, consider what photos, illustrations or other graphics can go with the text. These are essential. But DO NOT include them in your text. Instead, put notations in the text such as “Insert Photo #1 here.” Send the illustrations separately (and numbered as you would want them in the text, for example: “Belida1.jpg” or “PhotoEtch1.jpg”). JPG, GIF, or PNG formats are best for pictures.

If you have captions you would prefer for your illustrations, please create a separate text for captions only, each of which should be linked to a specific numbered picture. For example, “‘The front grill has been cut away from the engine’—with illustration #2.”

(Please note when the article is being prepared for insertion in the Flyer, pictures sometimes have to be relocated.)

Also, don't forget to add a short biographical sketch and provide a headshot photo of yourself.

All articles are subject to editing. However, if any edit might affect the substance or meaning of your text, you will be asked if the proposed change preserves the accuracy of the item. ☒

Did You Know? The Potomac Division Has Teams

No, not softball or anything like that. These are teams in the sense that Potomac Division members can link up with fellow members in the same geographic area. Our goal is to assist small groups with things like carpooling to open houses and other model railroad events.

Many members have complained that our Division is lacking in opportunities for socializing. The teams concept is designed to promote camaraderie. That could lead to operating sessions, helping with layout construction, workshops, and non-modeling interaction.

Our premise is that members who live near each other are more likely to become more active if there are other model railroaders nearby who share their interests.

Here's the catch: it's completely voluntary. You have to opt in. And if you were worried about this: there are no dues.

We already have a list of local team coordinators. If you opt in, you'll get the contact information for others on your team. You just might discover that there is a Potomac Division member right down the street from you. So sign up by e-mailing your team leader (if you haven't already).

Here are the coordinators we have in place:

Virginia

Last Name	First name	Community	County	Phone	Email
Stanford	Bradley	Arlington	Arlington & Alexandria	703-559-1166	stanford@smart.net
Stanley	Jerry	Hume	Fauquier	540-364-1815	jerry@madisonhomesinc.com
Boughton	Roger	Goldvein	Fauquier	540-455-2205	BoughtonR@aol.com
Stoermer	Lee	Aldie	Loudoun	928-580-3209	leetrains@yahoo.com
Paganoni	John	Manassas	Prince William	703-791-5055	john.paganoni@comcast.net
Mosteller	William	Fairfax	Fairfax	703-272-8190	wsm@greatdecals.com
Phillips	Nigel	Reston	Fairfax	703-901-7636	nigelp18000@gmail.com

Maryland

Last Name	First name	Community	County	Phone	Email
Latham	Dale	Waldorf	Charles	301-645-3055	dale.latham@verizon.net
Vacant			Montgomery	Please step up and fill this slot	

Where two individuals are listed as team leaders, they should be considered as co-team leaders.

What's Changing on the Website

When you visit the Potomac Division website <http://potomac-nmra.org/> you'll see some changes. Web designer Marshall Abrams and webmaster Bill Mosteller are continuously improving and upgrading the site. The work to implement these changes has to fit it with their other activities, so the improvements are appearing incrementally rather than as a big new rollout.

Home Page

The first change you'll see is the new home page. For the time being, the menu on the left remains unchanged. But the main frame has been completely replaced. This page is mobile-friendly, meaning that it can be viewed on the small screen of a smartphone or tablet. If you don't have one of these devices, the effect can be seen by narrowing the browser window on your computer.

The logos and title at the top of the page, and the two columns below, reduce in width to accommodate the width of the viewing device, the switch to single-column as necessary.

The slide show of home layouts was derived from the 2018 MER convention. If you'd like to add your layout, get in touch with webmaster@potomac-nmra.org.

Current announcements and features are presented in colored boxes in the right column, with links to more information.

The one holdover from the prior home page is the Metro train scooting across the screen. Under the train are links to information about Potomac Division, contact information for officers and officials, and links to the Mid-Eastern Region and National NMRA websites.

Search Back Issues of the *Potomac Flyer*

The *Potomac Flyer* is a marvelous resource. There's lots of good information in issues going back to the year 2000. Until now, finding that information has been difficult. No more! We've installed Custom Google Search at the top of the Flyer page (which is accessed from the menu). This instance is configured to search only the back issues of the *Flyer*. Google serves up the results of the search in a separate window. There are ads at the top; we may be able get an ad exemption for PD, but that's work in progress. 

Invitation to Authors and Photographers

by Jack Dziadul, Director, Mid-Eastern Region

Do you have a modeling tip, a how-to article, a step-by-step scratch-build, kit-bash, or kit build to share with your 1,800 friends in the Mid-Eastern Region? Perhaps you have photo exhibits of a recent train show, from a layout tour, or of an operating session. With our e-Local, we have an expanded page count available to publish your contribution and sharp color to give some pop to your photographs.

Among our Region's objectives are:

- Expand interest in and publicize the hobby of model railroading
- Promote membership growth (and retention)
- Support the National Achievement Program.

Each member can play a role in achieving these objectives.

We have a great deal of talent in the MER as evidenced by so many Master Model Railroaders in the Region, and in the variety of high-quality contest entries at our conventions. Let us all share those talents more broadly.

For those participating in the Achievement Program and working toward their Railroad Author Certificate and MMR, the Region and Division level requirements below are excerpted from the NMRA web site. Refer to www.nmra.org [Education tab](#) for the full statement of requirements that encompass clinics, etc.

To qualify for this certificate, you must:

Prepare and submit material on any of the following subjects:

- Model Railroading.
- Prototype Railroading, Applicable to Modeling.
- NMRA Administration (e.g. Officers or Committee Reports).

The material being claimed must be the work of the author, photographer, artist, draftsman, etc. applying for the certificate. A total of forty-two (42) points must be earned. This is material that appears in printed media, such as newsletters and may include text, photographs, drawings, etc. Material that has been published or accepted for publication may be claimed. A copy of the published material or of the acceptance receipt from the publication must be submitted as proof.

Item	Region	Division
Article or Column	2	1
Photos or Art Work	2	1
Scale Drawing of Prototype	4	2
Scale Drawing of Track Plan	2	1

- A "page" is approximately 1200 words. Credit may be claimed for partial pages down to quarter pages.
- No more than half of the total required points (21) may be claimed for Division or 100% NMRA Club publications.

You will note that your article does not need to actually even be published before submitting your Region level points for credit. You only need certification from the Editor that your work has been accepted for publication. For guidance or questions pertaining to the Achievement Program, please contact Region AP Coordinator Dave Chance or your Division AP Coordinator. (Ed: Mat Thompson is the [PD AP Coordinator](#)).

What is the next step?

Submit your article, photographs or inquiries to Editor Clint Hyde (local-editor@mer-nmra.com) with a copy to Jack Dziadul (jdziadul@mer-nmra.com). Other information including the publication schedule and deadlines can be found in *The Local*. ☒

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Layout Tour Report

Jim Zarnick's Butler and Western New York Railroad

Photos and article by Ernie Little

Saturday, March 30th found members of the Potomac Division attending a tour of Jim Zarnick's Butler and Western New York railroad at his home in Ashburn, Virginia. Thanks to Jim for taking the time to share his layout with the members of the Division in attendance.

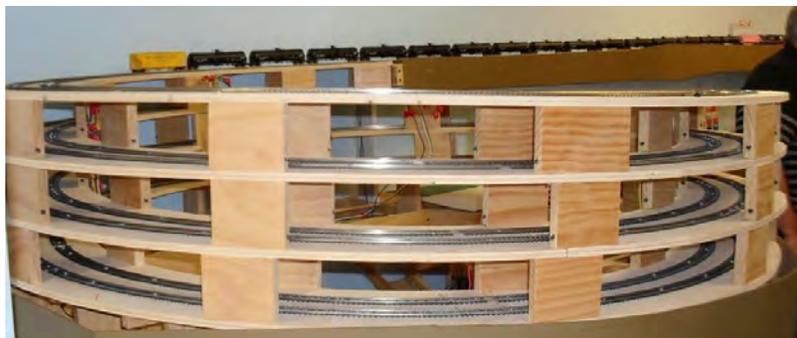
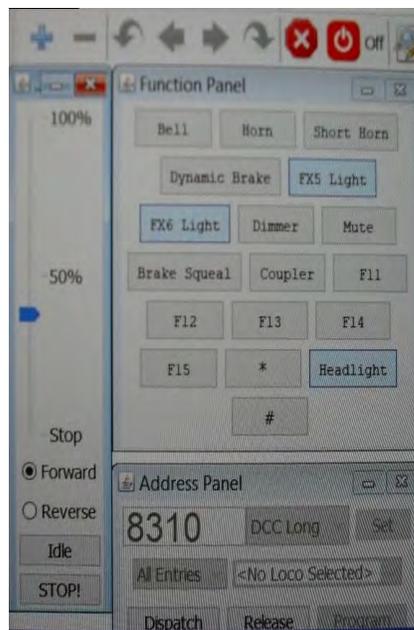
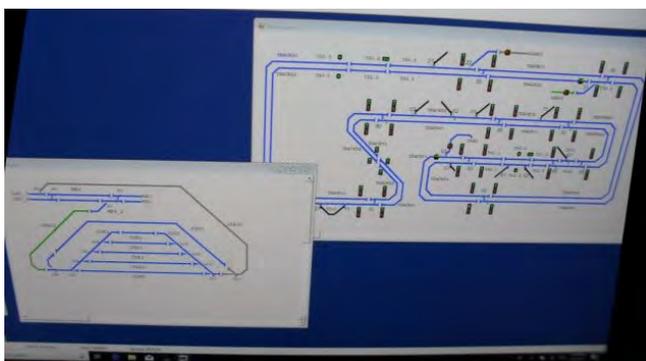
Jim started construction on this layout in 2013 and is continuing to work on completing the track and scenery. His layout models the modern era using diesel locomotives, although he is contemplating having one steam locomotive on the layout at some time in the future.

This HO scale layout was designed using CADRAIL to be double track with continuous-running design. The layout is double-decked, of 12 feet by 35 feet in size, has a helix, and has 225 feet (scale 3.5 miles) of track. The base of the layout is a combination of freestanding platform and support members attached directly to the wall using 1"x4" pine and 1/2" plywood.

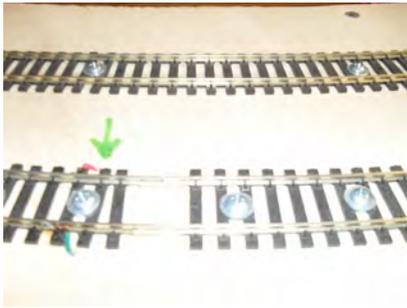
The trackbed is a combination of an underlayment material for floors and cork. This combination appeared to work very well for reducing the sound of trains moving across the track. Jim used Micro Engineering flex track for the mainline and sidings on the layout. The main line is code 83,

code 100 in the hidden staging area, and code 70 on the sidings and yard. One thing of note is that Jim used caulk, not track nails, to secure his track in place. The switches used on the layout are manufactured by Shinohara and Peco. Jim used Tortoise motors and a few ground throws for switch movement.

The layout uses CTI electronics to control the mainline signaling, with



JMRI software connected to the NCE operating system to provide track power and control of the locomotives.



The helix is a four-level design using 1/2" plywood and 1"x4" supports. The size of the helix is approximately 5 feet in diameter with 28" and 30" radius curves for the inside and outside track. The grade on the helix appeared to be easily handled by the diesel locomotives.

He chose to use small screws and washers on

each side of the rail joiners to secure the track to the roadbed.

An excellent under-construction layout and something we will have to revisit in the future to see Jim's progress. ☒



Ernie Little is a retired Battalion Chief of the Prince William County, VA Department of Fire and Rescue and resides in Manassas, VA. He has had an interest in model railroading

since the mid 1990's and has a 12 foot by 20

foot HO scale layout, the Norfolk Southern Connector, which is freelanced and represents a connector railroad that runs between two major railroads.

Layout Tour Report

Progress and Potential: Dick Kafka's Colorado Midland

by Bill Mosteller

Nick Kalis, our layout tour coordinator, likes to say that hosting an open house is an excellent vehicle for getting your layout shaped up, and our host for the April 27 tour, Dick Kafka, agrees completely. Much of what guests saw is recent, but exquisite work on his part.

The Colorado Midland stretched hundreds of miles from Colorado Springs to Grand Junction, most of the width of Colorado. Dick's layout focuses on Basalt, roughly at the midpoint of the line, with double-ended staging tracks beneath serving to simulate the un-modeled parts of the railroad.

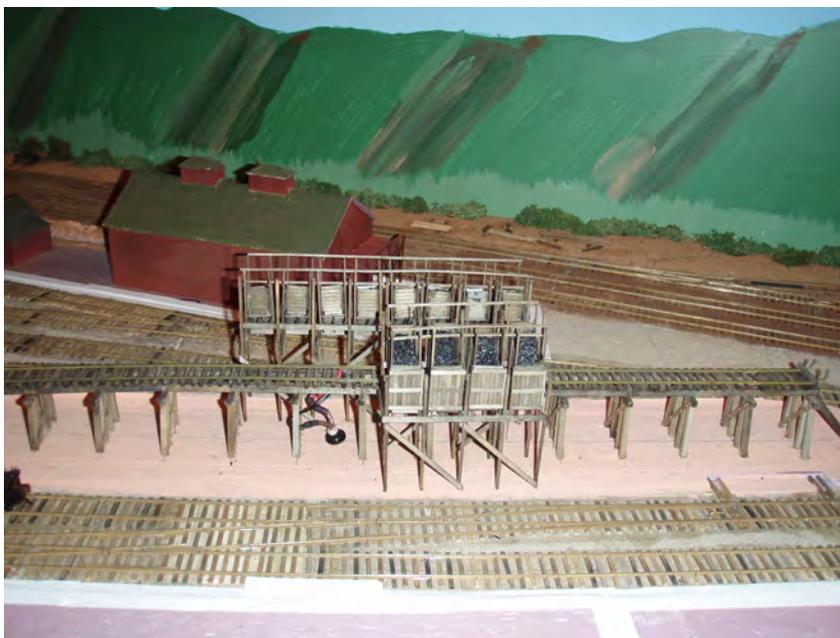
Basalt hosts scratchbuilt structures that produce traffic for the railroad, including a hotel, a station, an engine maintenance facility, an ice house, and a turntable. The railroad was built for the purpose of hauling coal, and the centerpiece of the Basalt trackage is a coal trestle.

Prototypically, the slope of the track is steep, suggesting only a few cars were moved up



at a time. Dick says that at the time he's modeling, hoppers were unloaded by men with shovels, forget rotary dumpers or even doors on the bottom of the hoppers. Basalt was a farming community on the western side of the continental divide, so reefers loaded with produce would be iced, and stock cars would be used to move cattle between winter and summer pastures, or to market. A yard provides space for these cars.

The handlaid track in Basalt, on wooden ties with spikes on every second tie, immediately caught my eye. After seeing this excellent engineering, I wasn't surprised to see a certificate on the wall indicating that Dick



is an IEEE (Institute of Electrical and Electronics Engineers) Fellow, retired from PEPCO. The staging track beneath has LEDs to illuminate the track, greatly improving visibility in that area. (The lighting reminded me of subway tracks in my native Boston.) The layout design was reviewed by John Armstrong prior to construction and he recommended the turnouts be placed along the front edge of the hidden track area. While this change makes the staging look a little less like a real railroad yard, and shortens some of the tracks more, it also means that maintenance and problem solving on the yard will be possible. Excellent idea!

Future plans include construction of the Aspen Branch, which at present is represented by a section of track leading off into the air. When this is completed, it will be possible to exchange freight between non-staging parts of the railroad. Operating sessions are already being held, and the extension will enhance them. ☒

[Return to Bill of Lading](#)

Bill Mosteller moved to the Washington, DC, area after college and has been here since, which led to an interest in the Virginian and Washington & Old Dominion, including decal offerings at www.greatdecals.com. Bill also conducts an hour-long clinic on Kadee couplers and offers what he considers the best coupler gauges in HO-, S-, and O-scale.



Layout Tour Report

Gil Fuchs' Haifa & Jerusalem Railroad

Photos by Marshall Abrams

Visitors to Gil Fuchs' Haifa & Jerusalem Railroad enjoyed seeing and discussing prototypes and models not often seen in this country. Israel Railways' 1950s legacy roster included German rolling stock as part of German WW II reparations, Turkish equipment from the pre-WW I Ottoman Empire, British and other European equipment from the post-WW I Mandate, and American exports sold worldwide.

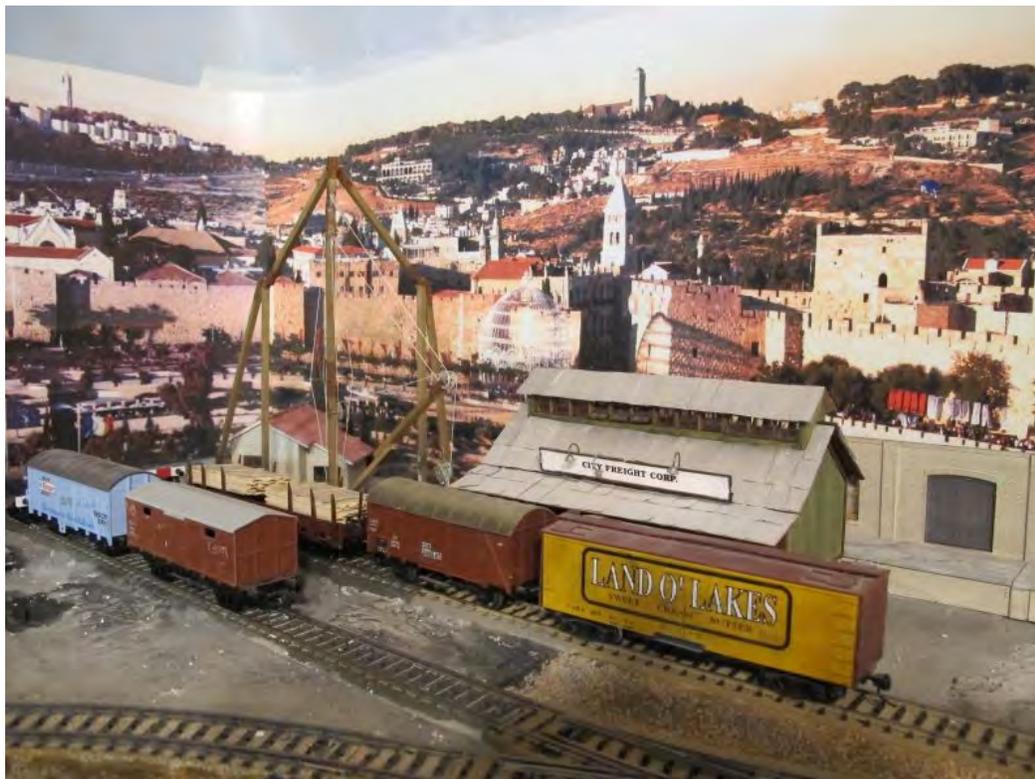
The HO-scale model started as Märklin three-rail AC (like Lionel, except that the center "rail" is made up of nearly-invisible metal studs in the middle of the ties), which Gil converted to DCC (not Märklin proprietary digital protocol). Many of Gil's engines are Märklin models, which he converted to DCC. Gil is on the NMRA DCC committee and can't



resist making his own decoders. He also has a few Atlas locomotive mechanisms where he has replaced the American-prototype plastic body shells with suitable Israeli models that he is fabricating on his two 3D printers.

Gil's hand-out for visitors notes the Haifa & Jerusalem Railroad is a fictitious and freelanced representation of Israel Railways' coastal network

during the early 1950's. Dimensions are approx. 26' x 14' with 2 levels. This allows Gil to mix steam and diesel.



The rolling stock of Israel Railways in its early days was unique as it was custom built by various foreign manufacturers, mostly of British, American and German origin, operated for a relatively long period. The Baldwin 4-6-0s, for example, served from 1919 until 1960. By 1950 most of the steam and rolling stock had undergone

conversion and looked nothing like its original form. This means most of the models had to be built from scratch or kitbashed at best. Gil's equipment is comprised mostly of Märklin models of European prototypes, and he is gradually adding Israeli prototypical models.

The layout is about 50% scenicked. Structures are mostly representative of the area and period. Backdrops are a mix of airbrush drawn scenery, cutout buildings and panoramic photographs. The view of Jerusalem (on the cover) is striking, including the Dome of the Rock and the King David Hotel. (Gil said he gave Staples a hard time getting it printed the way he wanted.)

There is a good representation of self-designed, 3D printed structures and model cars. The layout has eight towns or locations, each with a few industries. He uses switch lists generated by JMRI Operations software. Most of his engines carry DCC decoders which Gil designed and built, including several sound decoders. Turnouts are DCC controlled by self-designed and built accessory decoders.

The command station and throttles are Digitrax Chief. Märklin universal motors were modified to permanent magnet DC, and direction control changed to use the decoder. As feedback from operators about the Märklin loop coupler was not positive, all the rolling stock was upgraded with Kadee Knuckle couplers. Kadee #308 Magnets for uncoupling were added at strategic points, which required modification of the track (cuts in the roadbed) and the magnets (milling a slot to accommodate the 3rd rail). The aging, Märklin metal (M) track had caused poor rail contact and was replaced with the improved, nickel silver Märklin C track. The dark grey basalt rock ballast built into the new track did not fit the prototype, so it was all spray-painted in tan color. (R)

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Coming Layout Tour

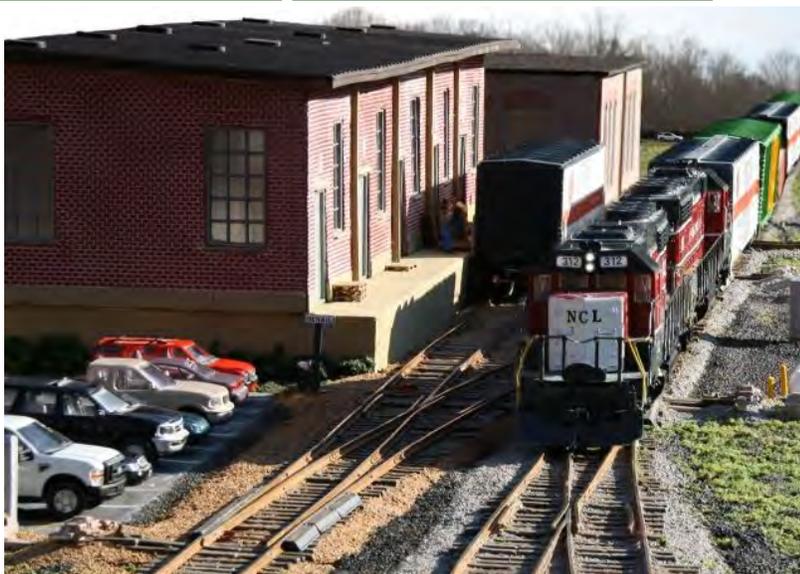
Bob Rodriguez's Nickel City Line

When: Saturday, June 22, 2019, 1:00 PM to 4:00 PM

Where: Addresses are not published on the web in order to protect hosts' privacy.

Access: The layout is entered from the interior stairway. It is not handicapped accessible.

A modern, turn of the century HO Scale model railroad set in the Allegheny Mountains of Pennsylvania in the late 1990s. The railroad is a free-lanced short line railroad which operates 102 miles of single track mainline between Harrisburg and Ridgeway, Pennsylvania. Leased trackage rights extend the line west to Pittsburgh and Erie, Pennsylvania. The layout is medium size, filling an 18' x 22' space in the basement. A new 7' x 24' section of triple deck layout expansion is under construction in the adjacent room. The layout is operations-focused, hosting 6 to 9 operating sessions per year. Operations include freight, coal, local freight, passenger, and commuter service running against a 2:1 fast clock. The layout is controlled by Digitrax DCC connected to several computers running JMRI software. JMRI software runs the Computer Aided Dispatch and Yardmaster computers, signals, route selection, train detection, and fast clocks. ☒



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Coming Layout Tour

Douglas Hess' Ohio Central RR

When: Saturday July 20, 2019, 1 - 4 PM

Where: Addresses are not published on the web in order to protect hosts' privacy.

Access: Entry into the 11'x13' train room is via a 42" duck-under. The basement is accessed by a stairway that is not handicap-friendly. However, if weather has been dry for a day or two, the basement also may be reached down a short gently sloping lawn.

Set in the gently rolling hills of east central Ohio, this HO-scale layout represents a freelanced section of the short line Ohio Central Rail Road (OHCR) about the turn of the 21st century. This is an around-the-walls shelf switching layout with an interchange off of the Norfolk Southern and a branch running to a small stub yard in a mid-sized city where local industries are served. The Wheeling & Lake Erie has trackage rights to serve a small coal-fired power plant. Trackwork has been laid and is operational. Scenery is in very early stages of completion. Digitrax DCC is installed.

This is PD's first visit to this railroad. ☒

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Coming Layout Tour

Paul Hutchins' Modern-Day CSX

When: Saturday, August 17, 2019, 1 to 4 pm

Where: Addresses are not published on the web in order to protect hosts' privacy.

Access: My layout is in my garage so it is handicapped accessible.

I model current era CSX, Conrail, and Norfolk Southern in HO scale. I call my railroad the "P&D Railroad." It's close to a circle eight horizontally, and in a dogbone vertically. My operating system is Digitrax.

This is PD's first visit to this railroad. ☒



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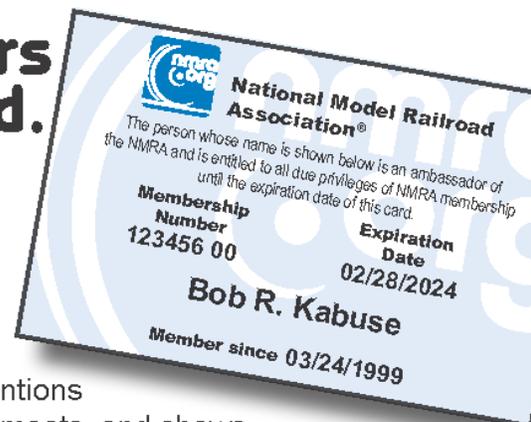
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